



## SPECIFICATIONS, STANDARDS AND SPECIAL PROVISIONS

- ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" ("STANDARD SPECIFICATIONS"), ADOPTED APRIL 1, 2016; THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", ADOPTED JANUARY 1, 2019; THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS" (IMUTCD); THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER CONSTRUCTION IN ILLINOIS", 7TH EDITION, 2014; THE LATEST EDITION OF THE "ILLINOIS URBAN MANUAL"; THE DETAILS IN THE PLANS; AND THE SPECIAL PROVISIONS AND IDOT STANDARD DRAWINGS INCLUDED IN THE CONTRACT DOCUMENTS.
- ALL REFERENCES TO "ENGINEER" SHALL BE INTERPRETED AS THE RESIDENT ENGINEER.
- ALL UTILITY COMPANIES, SCHOOL DISTRICTS, AND LOCAL POLICE AND FIRE DEPARTMENTS SHALL BE NOTIFIED BY THE CONTRACTOR AT LEAST 72 HOURS PRIOR TO THE START OF CONSTRUCTION.

## STAKING

- ALL RADII FOR PROPOSED CURB AND GUTTER ARE TO THE EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- THE STATION/OFFSET/ELEVATIONS NOTED FOR ALL DRAINAGE STRUCTURES LOCATED IN THE CURB LINE REFER TO THE POSITION OF THE ADJACENT EDGE OF PAVEMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING THE OFFSET NECESSARY FOR EACH STRUCTURE TO SET THE FRAME AND GRATE IN THE PROPER LOCATION. ALL OTHER STRUCTURES ARE DIMENSIONED TO THE CENTER OF STRUCTURE, UNLESS OTHERWISE NOTED.
- PAVEMENT GRADES: THE ELEVATIONS INDICATED ON THE PLANS ARE FINISHED GRADES OF PROPOSED PAVEMENT, UNLESS OTHERWISE INDICATED.
- THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS OR PROPERTY OR REFERENCE MARKERS UNTIL THE VILLAGE, ITS AGENT OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.

## STAKING (LAST 4 BLOCKS OF PLYMOUTH STREET)

- ALTHOUGH SURVEY HAS NOT BEEN COMPLETED FOR THESE SECTIONS OF ROADWAY, A PROPOSED CENTERLINE HAS BEEN SHOWN. IN GENERAL, THE CENTERLINE REPRESENTS THE CENTER-OF-ROADWAY AND THE CENTER-OF-ROW. THE PROPOSED CENTERLINE IS ONLY A BEST-FIT APPROXIMATION BASED ON AERIAL IMAGERY. ITS PURPOSE IS ONLY TO PROVIDE A GENERAL LENGTH OF ROADWAY IMPROVEMENTS.
- THE DIMENSIONS SHOWN IN THE PLANS ARE APPROXIMATE BASED ON FIELD INVESTIGATIONS. BY NO MEANS SHOULD THE DIMENSIONS ACT AS AN ABSOLUTE LIMIT OF PROPOSED WORK. THE FINAL LENGTHS AND AREAS OF PROPOSED WORK WILL BE DETERMINED BY THE ENGINEER IN THE FIELD.

## PAVING, CURB & GUTTER, AND SIDEWALK

- THE PAVEMENT PATCHING AND CURB AND GUTTER REMOVAL AND REPLACEMENT LOCATIONS SHOWN IN THE PLANS ARE ONLY APPROXIMATIONS MADE DURING THE DESIGN PROCESS BASED ON FIELD INVESTIGATIONS. THE ENGINEER IN THE FIELD SHALL MAKE THE FINAL DETERMINATION ON THE LOCATION OF PAVEMENT PATCHES AND CURB AND GUTTER REMOVAL AND REPLACEMENT.
- THE CONTRACTOR SHALL SAW CUT PAVEMENT, CURB & GUTTER, DRIVEWAY AND SIDEWALK AS INDICATED ON THE PLANS TO SEPARATE THE EXISTING MATERIAL TO BE REMOVED BY MEANS OF AN APPROVED SAW TO FULL DEPTH AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. THIS WORK SHALL BE INCLUDED IN THE COST OF THE ITEM BEING REMOVED. THE CONTRACTOR SHALL BE REQUIRED TO SAW VERTICAL CUTS SO AS TO FORM CLEAN VERTICAL JOINTS. SHOULD THE CONTRACTOR DEFACE ANY EDGE, A NEW SAWED JOINT SHALL BE PROVIDED AND ANY ADDITIONAL WORK, INCLUDING REMOVAL AND REPLACEMENT, SHALL BE DONE AT CONTRACTOR'S OWN EXPENSE.
- BINDER COURSE SHALL NOT BE PLACED ADJACENT TO CURB AND GUTTER UNTIL THE CURB AND GUTTER HAS BEEN PROPERLY CURED AND BACKFILLED TO THE SATISFACTION OF THE ENGINEER.
- HOT-MIX ASPHALT SURFACE COURSE SHALL NOT BE PLACED IN A STAGE UNTIL ALL EARTH EXCAVATION, TOPSOIL PLACEMENT, AND HOT-MIX ASPHALT BINDER COURSE WITHIN THE STAGE HAVE BEEN COMPLETED TO THE SATISFACTION OF THE ENGINEER.
- THE THICKNESSES OF HOT-MIX ASPHALT MIXTURES SHOWN ON THE PLANS ARE NOMINAL. DEVIATIONS MAY OCCUR DUE TO IRREGULARITIES IN THE BINDER OR BASE UPON WHICH THE HOT-MIX ASPHALT MATERIALS ARE PLACED.
- THE FINAL EDGE-OF-PAVEMENT SURFACE ELEVATION SHALL BE ¼" ABOVE THE GUTTER AS SHOWN IN IDOT HIGHWAY STANDARD 606001.
- ANY MODIFICATIONS REQUIRED IN THE PLACEMENT OF COMBINATION CONCRETE CURB AND GUTTER WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF "COMBINATION CONCRETE CURB AND GUTTER, TYPE AS SPECIFIED". THIS INCLUDES, BUT IS NOT LIMITED TO, TRANSITIONS IN THE CURB HEIGHT OR GUTTER WIDTH TO MATCH EXISTING CONDITIONS, TRANSITIONS AT DRAINAGE STRUCTURES, AND IRREGULARITIES AT EDGE-OF-PAVEMENT.
- ALL PROPOSED SIDEWALK AND SIDEWALK CURB RAMPS SHALL BE CONSTRUCTED TO THE SLOPES AND GRADES SHOWN IN IDOT HIGHWAY STANDARD 424001. THE PROPOSED SIDEWALK LIMITS SHOWN IN THE PLANS ARE ONLY APPROXIMATIONS BASED ON FIELD INVESTIGATIONS. THE ENGINEER WILL MAKE THE FINAL DETERMINATION OF THE SIDEWALK REPLACEMENT LIMITS IN THE FIELD. THE 6 INCH SIDE CURB ADJACENT TO SIDEWALK RAMPS WILL NOT BE PERMITTED. INSTEAD, THE LANDSCAPED AREAS NEAR THE SIDEWALK RAMPS WILL BE REGRADED TO MATCH THE PROPOSED EDGE-OF-SIDEWALK. SPOT REPAIRS WILL BE DETERMINED BY THE ENGINEER IN THE FIELD TO ADDRESS TRIP HAZARDS AND COMPLY WITH ADA GUIDELINES.

## EXCAVATION

- EXCAVATION BENEATH PAVEMENT PATCHES AS SHOWN IN THE PLANS SHALL ONLY BE DONE IF THERE IS NO EXISTING AGGREGATE BASE COURSE OR IF THE EXISTING AGGREGATE BASE COURSE IS DETERMINED TO BE UNSTABLE. THE FINAL CONDITION OF THE MATERIAL WILL BE DETERMINED BY THE ENGINEER IN THE FIELD. THE EXCAVATION SHALL BE A MINIMUM OF 4" BENEATH THE PAVEMENT PATCH WHICH VOLUME, MEASURED IN CUBIC YARDS, WILL BE REPLACED WITH AGGREGATE BASE COURSE, TYPE B.

## DRIVEWAY RECONSTRUCTION

- UNLESS DRIVEWAY RECONSTRUCTION IS INDICATED IN THE PLANS, THERE IS NO ANTICIPATION TO RECONSTRUCT ALL THE DRIVEWAY ENTRANCES. HOWEVER, IF A SITUATION ARISES WHERE DRIVEWAY RECONSTRUCTION IS NEEDED TO COMPLETE THE WORK SHOWN IN THE PLANS, PRIOR APPROVAL SHALL BE OBTAINED FROM THE ENGINEER BEFORE STARTING WORK. THE ENGINEER SHALL MAKE THE FINAL DECISION ON THE LIMITS OF DRIVEWAY RECONSTRUCTION. ANY DRIVEWAYS THAT WERE DAMAGED WITHOUT APPROVAL FOR RECONSTRUCTION FROM THE ENGINEER OR DAMAGED DUE TO CONTRACTOR'S NEGLIGENCE SHALL BE REPLACED IN KIND AT CONTRACTOR'S OWN EXPENSE.

- WHEN PERMITTED BY THE ENGINEER, DRIVEWAY RECONSTRUCTION SHALL BE CONSTRUCTED AS FOLLOWS:

RESIDENTIAL HMA DRIVEWAY RECONSTRUCTION:  
 -HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 4"  
 -(INCLUDES AGGREGATE BASE COURSE, TYPE B 6")  
 RESIDENTIAL PCC DRIVEWAY RECONSTRUCTION:  
 -PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH, SPECIAL  
 -(INCLUDES AGGREGATE BASE COURSE, TYPE B 4")  
 COMMERCIAL HMA DRIVEWAY RECONSTRUCTION:  
 -HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 6"  
 -(INCLUDES AGGREGATE BASE COURSE, TYPE B 8")  
 COMMERCIAL PCC DRIVEWAY RECONSTRUCTION:  
 -PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH, SPECIAL  
 -(INCLUDES AGGREGATE BASE COURSE, TYPE B 6")

## SEWER

- THE COST OF MAKING SEWER CONNECTIONS TO EXISTING OR PROPOSED SEWER OR DRAINAGE/SANITARY STRUCTURES SHALL BE INCLUDED IN THE COST OF THE SEWER OR STRUCTURE BEING CONSTRUCTED, RECONSTRUCTED, OR ADJUSTED.
- UNLESS OTHERWISE NOTED ON THE PLANS, THE EXISTING DRAINAGE FACILITIES SHALL REMAIN IN USE DURING THE PERIOD OF CONSTRUCTION. LOCATIONS OF EXISTING DRAINAGE STRUCTURES AND SEWERS AS SHOWN ON THE PLANS ARE APPROXIMATE. PRIOR TO COMMENCING WORK THE CONTRACTOR, AT HIS OWN EXPENSE, SHALL DETERMINE THE EXACT LOCATIONS OF EXISTING STRUCTURES WHICH ARE WITHIN THE PROPOSED CONSTRUCTION LIMITS. DURING CONSTRUCTION, IF THE CONTRACTOR ENCOUNTERS OR OTHERWISE BECOMES AWARE OF ANY SEWERS, UNDERDRAINS OR FIELD DRAINS WITHIN THE RIGHT-OF-WAY OTHER THAN THOSE SHOWN ON THE PLANS, HE SHALL SO INFORM THE ENGINEER, WHO SHALL DIRECT THE WORK NECESSARY TO MAINTAIN OR REPLACE THE FACILITIES IN SERVICE AND TO PROTECT THEM FROM DAMAGE DURING CONSTRUCTION IF MAINTAINED. EXISTING FACILITIES TO BE MAINTAINED THAT ARE DAMAGED BECAUSE OF THE NON-COMPLIANCE WITH THIS PROVISION SHALL BE REPLACED AT THE CONTRACTOR'S OWN EXPENSE, AND THE WORK SHALL BE DONE ACCORDING TO SECTIONS 550 AND 601, AND ARTICLE 104.02 OF THE STANDARD SPECIFICATIONS.
- WHEN EXISTING DRAINAGE FACILITIES ARE DISTURBED, THE CONTRACTOR SHALL PROVIDE AND MAINTAIN TEMPORARY OUTLETS AND CONNECTIONS FOR ALL PRIVATE OR PUBLIC DRAINS, SEWERS OR CATCH BASINS. THE CONTRACTOR SHALL PROVIDE FACILITIES TO TAKE IN ALL STORM WATER WHICH WILL BE RECEIVED BY THESE DRAINS AND SEWERS AND DISCHARGE THE SAME. HE SHALL PROVIDE AND MAINTAIN AN EFFICIENT PUMPING PLANT, IF NECESSARY, AND A TEMPORARY OUTLET. HE SHALL BE PREPARED AT ALL TIMES TO DISPOSE OF THE WATER RECEIVED FROM TEMPORARY CONNECTIONS UNTIL SUCH TIME AS THE PERMANENT CONNECTIONS WITH SEWER ARE BUILT AND IN SERVICE. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT.
- ALL ABANDONED PIPE AND STRUCTURE INVERTS SHALL BE PLUGGED WITH BRICK AND MORTAR TO THE SATISFACTION OF THE ENGINEER. THIS WORK SHALL BE INCLUDED IN THE COST OF ITEMS BEING REMOVED.
- TOP OF FRAME ("RIM") ELEVATIONS GIVEN ON THE PLANS ARE ONLY TO ASSIST THE CONTRACTOR IN DETERMINING THE APPROXIMATE OVERALL HEIGHT OF EACH STRUCTURE. FRAMES ON ALL NEW STRUCTURES SHALL BE ADJUSTED TO THE FINAL ELEVATIONS OF THE AREAS IN WHICH THEY ARE LOCATED AS PART OF THE STRUCTURE COST. PIPE INVERTS AND DIRECTIONS NOTED IN THE PLANS ARE NOT ABSOLUTE AND MAY REQUIRE SLIGHT ADJUSTMENT IN THE FIELD AS DIRECTED BY THE ENGINEER.
- ALL EXISTING WATER SERVICES, SEWER SERVICES AND SEWER MAINS NOT IN DIRECT CONFLICT WITH THE PROPOSED DRAINAGE AND UTILITY INSTALLATIONS SHALL BE PROPERLY LOCATED AND PROTECTED DURING CONSTRUCTION. SHOULD THE CONTRACTOR CHOOSE TO MAKE SEWER AND WATER ADJUSTMENTS TO INCREASE PROPOSED DRAINAGE AND UTILITY PRODUCTION, THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE DONE AT CONTRACTOR'S OWN EXPENSE. ANY DAMAGE TO SAID SERVICES AND MAINS NOT CONSIDERED TO BE IN CONFLICT WITH THE PROPOSED DRAINAGE AND UTILITY INSTALLATIONS SHALL BE REPAIRED OR REPLACED AT CONTRACTOR'S OWN EXPENSE.
- SANITARY STRUCTURE AND DRAINAGE STRUCTURE REPLACEMENT SHALL INCLUDE THE RECONNECTION OF ALL SEWERS. EXISTING SEWERS THAT ARE DAMAGED DURING THE STRUCTURE REPLACEMENT PROCESS SHALL BE REPLACED IN KIND AND AT THE SAME DIAMETER. THE COST OF DOING THIS WORK WILL BE INCLUDED IN THE COST OF THE PROPOSED UTILITY STRUCTURE BEING INSTALLED WHICH INCLUDES SEWER REPLACEMENT OF UP TO 5' IN LENGTH.
- THE MATERIALS USED IN THE COMPOSITION OF TRENCH BACKFILL SHALL ONLY BE CA-6 VIRGIN MATERIAL. RECYCLED MATERIAL WILL NOT BE ALLOWED.

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## WATER

- THE CONTRACTOR SHALL GIVE THE VILLAGE A MINIMUM OF 48 HOURS NOTICE PRIOR TO BEGINNING ANY WATER UTILITY WORK, INCLUDING WATERMAIN SHUT OFFS.
- SERVICE LINES TO RESIDENTS CANNOT BE SHUT OFF FOR MORE THAN 8 HOURS PER DAY (9AM TO 5PM) AND NO MORE THAN 2 SHUT OFFS FOR ANY GIVEN PROPERTY.
- THE COST FOR REMOVING EXISTING WATERMAIN AND FITTINGS IN ORDER TO INSTALL PROPOSED WATER ITEMS SHALL BE CONSIDERED AS INCLUDED IN THE COST OF THE PROPOSED WATER ITEMS BEING INSTALLED AS SHOWN IN THE PLANS.
- BUFFALO BOXES (B-BOXES) SHALL BE ADJUSTED TO THE FINAL GRADE AND WILL BE KEYABLE AFTER THE COMPLETION OF FINAL LANDSCAPING OR FINAL PAVING (IN AREAS WHERE B-BOXES ARE LOCATED IN PAVED SURFACES). THE ADJUSTMENT TO THE FINAL GRADE WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED AS INCLUDED IN THE COST OF THE PROPOSED ITEMS BEING INSTALLED.

## MAILBOXES

- THE CONTRACTOR SHALL PROTECT AND RELOCATE ALL EXISTING MAILBOXES WHICH INTERFERE WITH THE WORK IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF ARTICLE 107.20 OF THE STANDARD SPECIFICATIONS. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT, AND ADDITIONAL COMPENSATION WILL NOT BE ALLOWED.

## CLEARING

- CONTRACTOR SHALL PAY SPECIAL ATTENTION TO ARTICLE 201.01(a) OF THE STANDARD SPECIFICATIONS. REMOVAL OF ALL OBSTRUCTIONS IN THE RIGHT-OF-WAY, THAT ARE NOT INCLUDED IN A SPECIFIC REMOVAL ITEM, SHALL BE CONSIDERED CLEARING AND INCLUDED IN THE COST OF THE CONTRACT. THIS SHALL INCLUDE, BUT IS NOT LIMITED TO, FENCES, WALLS, FOUNDATIONS, BUILDINGS, WOODEN POWER POLES, WOODEN PLANTERS, GATES, AND ALL VEGETATION, TREES, SHRUBS, ETC. LESS THAN 6" IN DIAMETER.

PLOT DRIVER = ...\\dm04986\con\_pdf\c1t  
 PEN TABLE = ...\\smb1\con\idot\_general.tbl  
 FILE NAME = ...\\3318-shr-grenno.tbl.dgn



USER NAME = mjp	DESIGNED - MJP	REVISED -
	DRAWN - MC	REVISED -
PLOT SCALE = 1:8000' / 1"	CHECKED - JRV	REVISED -
PLOT DATE = 6/21/2019	DATE - 06/21/19	REVISED -

## VILLAGE OF VILLA PARK

## 2019 STREET IMPROVEMENT PROJECT GENERAL NOTES & INDEX OF SHEETS

SCALE: N.T.S. SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DUPAGE	37	2
CONTRACT NO.				
		ILLINOIS	FED. AID PROJECT	

**UTILITIES**

1. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES. THE LOCATION OF PUBLIC OR PRIVATE UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE AND THE ENGINEER DOES NOT GUARANTEE THEIR ACCURACY. THE CONTRACTOR WILL BE REQUIRED TO ASCERTAIN THE EXACT LOCATION OF SUCH UTILITIES AND EXERCISE CARE DURING CONSTRUCTION OPERATIONS SO AS NOT TO DAMAGE THEM IN ACCORDANCE WITH THE SPECIAL PROVISIONS AND ARTICLE 107.31 OF THE STANDARD SPECIFICATIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL EXISTING UTILITIES SO THAT THEIR FACILITIES MAY BE LOCATED AND ADJUSTED OR MOVED, IF NECESSARY, PRIOR TO THE START OF THE CONSTRUCTION OPERATIONS.
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL ABOVE AND BELOW GROUND UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER OR THE UTILITY OWNER. THIS WORK SHALL BE AT CONTRACTOR'S OWN EXPENSE. THE CONTRACTOR SHALL NOTIFY ALL UTILITY OWNERS OF HIS CONSTRUCTION SCHEDULE AND SHALL COORDINATE CONSTRUCTION OPERATIONS WITH THE UTILITY OWNERS SO THAT RELOCATION OF UTILITY LINES AND STRUCTURES MAY PROCEED IN AN ORDERLY MANNER. NOTIFICATION SHALL BE IN WRITING, WITH COPIES TRANSMITTED TO THE ENGINEER.
3. THE CONTRACTOR SHALL RECEIVE NO ADDITIONAL COMPENSATION FOR CONSTRUCTION STAGING NECESSARY TO ACCOMMODATE UTILITY RELOCATION OR ADJUSTMENT AND/OR FOR DELAYS CAUSED BY UTILITY RELOCATION OR ADJUSTMENT.
4. THE CONTRACTOR SHALL FURNISH ALL LABOR, EQUIPMENT AND MATERIAL NECESSARY FOR DEWATERING TRENCH EXCAVATIONS AS WELL AS SHORING TRENCH WALLS DURING UTILITY OPERATIONS. DEWATERING OPERATIONS SHALL BE DONE THROUGH A FILTER SOCK TO PREVENT SOIL EROSION. COMPLIANCE WITH THE ABOVE WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT.

**SOIL EROSION AND SEDIMENT CONTROL**

1. ALL VEGETATIVE AND STRUCTURAL EROSION CONTROL PRACTICES SHALL BE CONSTRUCTED AND MAINTAINED IN ACCORDANCE WITH THE MINIMUM STANDARDS AND SPECIFICATIONS OF THE "ILLINOIS URBAN MANUAL (JUNE, 2013 EDITION)".
2. SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. SOIL STABILIZATION MEASURES SHALL CONSIDER THE TIME OF YEAR, SITE CONDITIONS AND THE USE OF TEMPORARY OR PERMANENT MEASURES. THE USE OF TEMPORARY EROSION CONTROL SEEDING WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT.
3. ALL EROSION CONTROL MEASURES SHALL BE IN PLACE BEFORE ANY WORK BEGINS.
4. THE CONTRACTOR SHALL INSPECT ALL EROSION CONTROL MEASURES WEEKLY AND AFTER EACH RAINFALL EVENT. ANY NECESSARY REPAIRS OR CLEANUP TO MAINTAIN THE EFFECTIVENESS OF SAID MEASURES SHALL BE MADE IMMEDIATELY. ALL MAINTENANCE OF EROSION CONTROL ITEMS IS INCLUDED IN THE COST OF THE ITEM PER ARTICLE 280.05 OF THE STANDARD SPECIFICATIONS.
5. ALL STORM SEWER FACILITIES THAT ARE OR WILL BE FUNCTIONING DURING CONSTRUCTION SHALL BE PROTECTED, FILTERED OR OTHERWISE TREATED TO REMOVE SEDIMENT. MUD AND SEDIMENT DEPOSITS SHALL BE REMOVED FROM THE ROADWAY AT THE END OF EACH WORK DAY BY SHOVELING AND/OR SWEEPING.
6. INLET FILTERS SHALL BE PLACED ON ALL CATCH BASINS, INLETS, AND MANHOLES WITH OPEN GRATES IN THE PROJECT AREA.
7. ALL SLOPES SHALL BE COVERED WITH SOD OR SEED & EROSION CONTROL BLANKET AS SOON AS GRADING AND PLACEMENT OF TOPSOIL HAS BEEN COMPLETED. IF PLACEMENT OF SOD OR SEED IS NOT FEASIBLE, CONTRACTOR SHALL UTILIZE OTHER SOIL STABILIZATION MEASURES.
8. STOCKPILES LEFT IN PLACE SHALL BE PROPERLY COVERED AND SECURED TO PREVENT ANY EROSION.
9. THE EROSION CONTROL MEASURES INDICATED ON THE PLANS ARE THE MINIMUM REQUIREMENTS. ADDITIONAL MEASURES MAY BE REQUIRED AS DIRECTED BY THE ENGINEER.

**TREE PROTECTION & LANDSCAPING**

1. THE ENGINEER WILL DETERMINE AREAS WHERE TEMPORARY FENCE MAY BE NEEDED TO PROTECT TREES IN CLOSE PROXIMITY TO CONSTRUCTION OPERATIONS. TEMPORARY FENCE SHALL BE PLACED AROUND THE TREE. NEW TEMPORARY FENCE OR THE RELOCATION OF TEMPORARY FENCE FROM ONE LOCATION TO ANOTHER LOCATION SHALL BE PAID FOR AS "TEMPORARY FENCE". THE TEMPORARY FENCE SHALL NOT BE RELOCATED UNTIL ALL WORK IS COMPLETED IN THAT AREA.
2. EXCAVATION NEAR TREES MAY REQUIRE TREE ROOT PRUNING AND/OR SOIL REMEDIATION AS DIRECTED BY THE ENGINEER.
3. A CONTINGENCY QUANTITY FOR TREE REMOVAL HAS BEEN PROVIDED IN THE CONTRACT. THE VILLAGE'S REPRESENTATIVE WILL DETERMINE WHICH TREE NEEDS TO BE REMOVED IN THE FIELD ACCORDING TO SECTION 201 OF THE STANDARD SPECIFICATIONS.
4. THE CONTRACTOR SHALL ADHERE TO LIMITS OF RESTORATION SHOWN IN THE PLANS. AREAS OUTSIDE THESE LIMITS THAT ARE DAMAGED OR DISTURBED BY THE CONTRACTOR SHALL BE RESTORED AT CONTRACTOR'S OWN EXPENSE, AND ADDITIONAL COMPENSATION WILL NOT BE ALLOWED.

**ADDITIONAL SYMBOLS, ABBREVIATIONS AND PATTERNS**

	EXISTING & PROPOSED VALVE VAULT
	EXISTING VALVE AND VALVE BOX
	EXISTING FIRE HYDRANT
	EXISTING DOMESTIC WATER SERVICE BOX
	EXISTING GAS VALVE
	DEPRESSED CURB (LOCATIONS SHOWN ON ROADWAY PLANS)
	DETECTABLE WARNING
	SIDEWALK LANDING AND TURNING SPACE AREA
	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH (REMOVAL PLAN) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (PROPOSED PLAN) LEVELING BINDER (MACHINE METHOD), N50 (PROPOSED PLAN)
	PORTLAND CEMENT CONCRETE SURFACE REMOVAL (VARIABLE DPETH)
	CLASS C PATCHES (DEPTH AS NOTED)
	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT
	PAVEMENT REMOVAL
	DRIVEWAY PAVEMENT REMOVAL
	SIDEWALK REMOVAL
	COMBINATION CURB AND GUTTER REMOVAL
	SINGLE ITEM REMOVAL
T.B.F.	TRENCH BACKFILL
	DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED FRAMES AND LIDS, TYPE 1, CLOSED LID
	DRAINAGE & UTILITY STRUCTURES TO BE RECONSTRUCTED FRAMES AND LIDS, TYPE 1, CLOSED LID
	DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED FRAMES AND GRATES, TYPE 11

**EPOXY COATING ON REINFORCEMENT**

1. ALL DOWEL BARS AND TIE BARS SHALL BE EPOXY COATED UNLESS OTHERWISE NOTED ON THE PLANS.

**SIGNING & STRIPING**

1. ALL EXISTING TRAFFIC SIGNS WHICH INTERFERE WITH THE CONTRACTOR'S WORK SHALL BE REMOVED PER ARTICLE 107.25. A RECORD SHALL BE MADE OF THEIR CONDITION, AND THEY SHALL BE SAFELY STORED AND SAFEGUARDED BY THE CONTRACTOR UNTIL THE ENGINEER DETERMINES THAT THEY BE REINSTALLED IN THE PERMANENT LOCATIONS. "STOP" SIGNS SHALL BE IMMEDIATELY REINSTALLED AFTER WORK IS COMPLETED IN AREA TO MAINTAIN ROADWAY SAFETY CONDITIONS. ANY SIGN WHICH IS DAMAGED DUE TO CONSTRUCTION ACTIVITIES SHALL BE REPAIRED OR REPLACED IN KIND AT CONTRACTOR'S OWN EXPENSE PRIOR TO PERMANENT REINSTALLATION.
2. ALL EXISTING SIGNS AND POSTS REMOVED AND NOT REINSTALLED WILL BE EVALUATED BY THE ENGINEER IN THE FIELD. THE ENGINEER WILL DETERMINE WHICH ITEMS WILL BE RETURNED TO THE VILLAGE AND WHICH ITEMS WILL BE DISPOSED. THE DELIVERY AND DISPOSAL OF THESE ITEMS WILL BE INCLUDED IN THE COST OF "REMOVE SIGN PANEL ASSEMBLY, TYPE AS NOTED". SIGNS AND POSTS DEEMED BY THE ENGINEER TO BE RETURNED SHALL BE DELIVERED TO:

VILLAGE OF VILLA PARK  
PUBLIC WORKS DEPARTMENT YARD  
51 SOUTH ARDMORE AVENUE  
VILLA PARK, IL 60181

**STATE STANDARDS**

STANDARD NO.	DESCRIPTION
000001-07	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
280001-07	TEMPORARY EROSION CONTROL SYSTEMS
420001-09	PAVEMENT JOINTS
420101-06	24' JOINTED PCC PAVEMENT
424001-11	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424021-05	DEPRESSED CORNER FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
602001-02	CATCH BASIN TYPE A
602011-02	CATCH BASIN TYPE C
602401-06	PRECAST MANHOLE TYPE A 4' DIAMETER
602601-06	PRECAST REINFORCED CONCRETE FLAT SLAB TOP
604001-04	FRAME AND LIDS TYPE 1
604006-05	FRAME & GRATE TYPE 3
604051-04	FRAME & GRATE TYPE 11
606001-07	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701011-04	OFF-RD MOVING OPERATIONS, 2L, 2W, DAY ONLY
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-08	TRAFFIC CONTROL DEVICES
720001-01	SIGN PANEL MOUNTING DETAILS
720006-04	SIGN PANEL ERECTION DETAILS
728001-01	TELESCOPING STEEL SIGN SUPPORT
780001-05	TYPICAL PAVEMENT MARKINGS

\* PRECAST REINFORCED CONCRETE FLAT SLAB TOPS WILL ONLY BE USED IF WARRANTED BY DEPTH RESTRICTIONS. OTHERWISE, ALL STRUCTURES SHALL BE EQUIPPED WITH CONES.

**NOTE: THE CONSTRUCTION DETAILS SHALL SUPERCEDE IDOT STANDARD DRAWINGS**

**MISCELLANEOUS**

1. DIMENSIONS: IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.
2. UNLESS OTHERWISE AUTHORIZED BY THE ENGINEER, ALL EXISTING ACCESS POINTS SHALL BE MAINTAINED AT ALL TIMES BY THE CONTRACTOR.

**GEOTECHNICAL AND CCDD REPORTS**

1. THOSE SEEKING THE FULL GEOTECHNICAL AND CCDD REPORTS SHOULD CONTACT THE OWNER OF RECORD. TO MAKE ARRANGEMENTS FOR ACCESS TO THIS INFORMATION, PLEASE CONTACT:

VILLAGE OF VILLA PARK  
PUBLIC WORKS DEPARTMENT  
PHONE: (630) 834-8505

PLOT DRIVER = ...\\vms04986\csm\poff\cst  
PEN TABLE = ...\\vms04986\csm\poff\pdu\tbl  
FILE NAME = ...\\3318-sht-gennoc02.dgn



USER NAME = mjp	DESIGNED - MJP	REVISED -
	DRAWN - MC	REVISED -
PLOT SCALE = 1:8000' / 1"	CHECKED - JRV	REVISED -
PLOT DATE = 6/21/2019	DATE - 06/21/19	REVISED -

**VILLAGE OF VILLA PARK**

**2019 STREET IMPROVEMENT PROJECT  
GENERAL NOTES & STATE STANDARDS**

SCALE: N.T.S. SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			37	3
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

**SUMMARY OF QUANTITIES**

SUMMARY OF 2019 STREET IMPROVEMENTS					REFERENDUM STREETS			NON-REFERENDUM STREETS				
IDOT CODE NO.	ITEM NO.	ITEM DESCRIPTION	UNIT	TOTAL QUANTITY	PLYMOUTH STREET (ARDMORE TO CHATHAM)	PLYMOUTH STREET (DOUGLAS TO ELLSWORTH)	PLYMOUTH STREET (FULTON TO VILLA)	PLYMOUTH STREET (CHATHAM TO DOUGLAS)	PLYMOUTH STREET (ELLSWORTH TO FULTON)	PARK BOULEVARD (PRINCETON TO ARDMORE)	PARK ALLEY (PRINCETON TO ARDMORE)	ST. CHARLES ALLEY (HARVARD TO YALE)
&	20100110	1	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	69	15	15	24	0	15	0	0
&	20100210	2	TREE REMOVAL (OVER 15 UNITS DIAMETER)	UNIT	108	16	24	24	20	24	0	0
	20101000	3	TEMPORARY FENCE	FOOT	1,360	520	320	120	80	80	80	40
*	20101200	4	TREE ROOT PRUNING	EACH	34	13	8	3	1	2	1	1
*	20101300	5	TREE PRUNING (1 TO 10 INCH DIAMETER)	EACH	7	1	2	0	1	2	0	1
*	20101350	6	TREE PRUNING (OVER 10 INCH DIAMETER)	EACH	51	11	6	18	4	7	3	0
*	20200100	7	EARTH EXCAVATION	CU YD	106	0	106	0	0	0	0	0
*	20201200	8	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	1,752	668	205	0	0	0	426	453
*	20800150	9	TRENCH BACKFILL	CU YD	40	0	5	0	0	0	0	15
	21001000	10	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQ YD	4,643	1,816	919	0	0	0	945	963
	25200200	11	SUPPLEMENTAL WATERING	UNIT	24	9	5	3	1	1	3	1
*	28000510	12	INLET FILTERS	EACH	31	9	6	7	0	2	5	1
*	30300001	13	AGGREGATE SUBGRADE IMPROVEMENT	CU YD	873	454	205	0	0	0	0	214
&	35101500	14	AGGREGATE BASE COURSE, TYPE B	CU YD	123	0	1	34	9	13	61	3
	35101800	15	AGGREGATE BASE COURSE, TYPE B 6"	SQ YD	3,328	1,513	762	0	0	0	524	529
*	40600290	16	BITUMINOUS MATERIALS (TACK COAT)	POUND	6,520	889	707	2,076	501	503	1,705	101
	40600827	17	POLYMERIZED LEVELING BINDER (MACHINE METHOD), 1L-4.75, N50	TON	366	34	34	130	32	32	104	0
*	40600982	18	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	328	82	70	88	0	0	88	0
	40603080	19	HOT-MIX ASPHALT BINDER COURSE, 1L-19.0, N50	TON	510	339	171	0	0	0	0	0
	40603335	20	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	1,252	261	175	345	84	84	282	15
	42000301	21	PORTLAND CEMENT CONCRETE PAVEMENT 8" (JOINTED)	SQ YD	1,053	0	0	0	0	0	0	524
	42001300	22	PROTECTIVE COAT	SQ YD	3,495	521	421	280	4	4	704	681
*	42400800	23	DETECTABLE WARNINGS	SQ FT	622	161	160	182	0	0	76	43
	44000100	24	PAVEMENT REMOVAL	SQ YD	3,638	1,600	823	0	0	0	0	614
	44000200	25	DRIVEWAY PAVEMENT REMOVAL	SQ YD	1,136	161	68	109	47	26	188	321
	44000500	26	COMBINATION CURB AND GUTTER REMOVAL	FOOT	2,178	466	277	256	20	20	952	79
	44000600	27	SIDEWALK REMOVAL	SQ FT	12,311	2,162	1,905	2,063	39	63	4,813	780
*		28	CLASS C PATCHES, 6 INCH	SQ YD	1,049	0	5	306	74	116	548	0
*		29	CLASS D PATCHES, 6 INCH	SQ YD	36	0	0	0	0	0	0	26
*		30	STORM SEWERS, RUBBER GASKET, CLASS A, 12"	FOOT	91	0	0	0	0	0	0	49
*	56104900	31	WATER VALVES 6"	EACH	1	0	0	0	0	0	1	0
*	56400500	32	FIRE HYDRANTS TO BE REMOVED	EACH	2	0	0	0	0	0	2	0
*	56400820	33	FIRE HYDRANT WITH AUXILIARY VALVE AND VALVE BOX	EACH	2	0	0	0	0	0	2	0
		34	CATCH BASINS, TYPE A, 4'-DIAMETER	EACH	2	1	0	0	0	1	0	0
		35	CATCH BASINS, TYPE C	EACH	4	0	1	0	0	0	1	1
		36	MANHOLES, TYPE A, 4'-DIAMETER	EACH	2	0	0	0	0	0	0	1
*	60248700	37	VALVE VAULTS, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	1	0	0	0	0	0	1	0
*	60404300	38	FRAMES AND GRATES, TYPE 3	EACH	1	1	0	0	0	0	0	0
*	60404800	39	FRAMES AND GRATES, TYPE 11	EACH	18	7	5	3	0	2	1	0
*		40	FRAMES AND LIDS, TYPE 1	EACH	11	2	2	1	0	1	1	2
*	66900200	41	NON-SPECIAL WASTE DISPOSAL	CU YD	426	0	0	0	0	0	0	426

\*INDICATES ITEM COVERED BY SPECIAL PROVISION

&INDICATES CONTINGENCY ITEM

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**VILLAGE OF VILLA PARK**

**2019 STREET IMPROVEMENT PROJECT  
SUMMARY OF QUANTITIES**

SCALE: N.T.S. SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			37	4
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

**SUMMARY OF QUANTITIES**

SUMMARY OF 2019 STREET IMPROVEMENTS					REFERENDUM STREETS			NON-REFERENDUM STREETS				
					PLYMOUTH STREET (ARDMORE TO CHATHAM)	PLYMOUTH STREET (DOUGLAS TO ELLSWORTH)	PLYMOUTH STREET (FULTON TO VILLA)	PLYMOUTH STREET (CHATHAM TO DOUGLAS)	PLYMOUTH STREET (ELLSWORTH TO FULTON)	PARK BOULEVARD (PRINCETON TO ARDMORE)	PARK ALLEY (PRINCETON TO ARDMORE)	ST. CHARLES ALLEY (HARVARD TO YALE)
IDOT CODE NO.	ITEM NO.	ITEM DESCRIPTION	UNIT	TOTAL QUANTITY	QUANTITY	QUANTITY	QUANTITY	QUANTITY	QUANTITY	QUANTITY	QUANTITY	
72000100	42	SIGN PANEL - TYPE 1	SQ FT	14	0	0	0	0	0	0	14	0
72400100	43	REMOVE SIGN PANEL ASSEMBLY - TYPE A	EACH	3	1	2	0	0	0	0	0	0
72400500	44	RELOCATE SIGN PANEL ASSEMBLY - TYPE A	EACH	3	1	2	0	0	0	0	0	0
72800100	45	TELESCOPING STEEL SIGN SUPPORT	FOOT	73	13	30	0	0	0	0	30	0
78000400	46	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	25	25	0	0	0	0	0	0	0
78000600	47	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	204	0	0	0	0	204	0	0	0
78000650	48	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	225	63	75	50	0	37	0	0	0
78001110	49	PAINT PAVEMENT MARKING - LINE 4"	FOOT	200	0	0	0	0	0	0	0	200
* 89502376	50	REBUILD EXISTING HANDHOLE	EACH	1	0	0	0	0	1	0	0	0
* Z0004514	51	HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 4"	SQ YD	229	131	28	44	0	26	0	0	0
* Z0004522	52	HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 6"	SQ YD	589	0	0	0	0	151	322	116	0
* Z0013798	53	CONSTRUCTION LAYOUT	L SUM	1								
* Z0017400	54	DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED	EACH	41	13	9	12	0	2	5	0	0
* Z0017700	55	DRAINAGE & UTILITY STRUCTURES TO BE RECONSTRUCTED	EACH	3	1	1	1	0	0	0	0	0
* Z0056604	56	STORM SEWER (WATER MAIN REQUIREMENTS) 8 INCH	FOOT	7	0	7	0	0	0	0	0	0
* Z0062700	57	SAWING P.C. CONCRETE PAVEMENT (FULL DEPTH)	FOOT	2,944	28	76	1,760	533	547	0	0	0
* X2110104	58	TOPSOIL FURNISH AND PLACE, 4" (SPECIAL)	SQ YD	2,232	893	550	270	60	70	274	22	93
* X2130010	59	EXPLORATION TRENCH, SPECIAL	FOOT	200	50	25	0	0	50	50	25	25
* X2520700	60	SODDING, SPECIAL	SQ YD	2,232	893	550	270	60	70	274	22	93
* 61	61	TEMPORARY ACCESS (DRIVEWAY ENTRANCE)	EACH	26	7	3	0	0	3	7	6	6
* X4023000	62	TEMPORARY ACCESS (ROAD)	EACH	10	4	2	0	0	0	2	2	2
* X4230710	63	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH, SPECIAL	SQ YD	220	59	49	65	47	0	0	0	0
* X4230800	64	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH, SPECIAL	SQ YD	355	0	0	0	0	0	80	275	275
* X4240430	65	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH, SPECIAL	SQ FT	11,092	2,371	2,136	2,400	0	0	3,533	345	307
* X4240460	66	PORTLAND CEMENT CONCRETE SIDEWALK 8 INCH, SPECIAL	SQ FT	1,306	0	0	0	0	0	818	488	0
* X4400100	67	PORTLAND CEMENT CONCRETE SURFACE REMOVAL (VARIABLE DEPTH)	SQ YD	5,930	736	723	2,985	742	744	0	0	0
* X4401198	68	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	SQ YD	2,691	0	0	0	0	0	2,512	126	53
* X6026624	69	VALVE BOXES TO BE ADJUSTED (SPECIAL)	EACH	1	0	0	0	0	0	0	1	0
* 70	70	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-4.12 (SPECIAL)	FOOT	860	0	0	0	0	860	0	0	0
* X6064200	71	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 (SPECIAL)	FOOT	2,854	1,380	820	256	20	25	95	79	179
* 72	72	CONCRETE WEDGE	FOOT	1,608	290	274	256	20	25	556	79	108
* X7010216	73	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1								
* XX003536	74	CONNECTION TO EXISTING WATER MAIN (NON PRESSURE)	EACH	3	0	0	0	0	3	0	0	0
* XX003668	75	PRECONSTRUCTION VIDEO TAPING	L SUM	1								
* 76	76	SANITARY MANHOLES, 4' -DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	1	0	0	0	0	1	0	0	0
* 77	77	WATER USAGE DEDUCTION	T GAL	100	30	10	20	5	5	10	10	10
* 78	78	WATER USAGE CREDIT	T GAL	100	30	10	20	5	5	10	10	10
* 79	79	CONTINGENCY ALLOWANCE	DOLLAR	50,000	9,000	3,000	6,000	4,000	4,000	8,000	8,000	8,000

\*INDICATES ITEM COVERED BY SPECIAL PROVISION

&INDICATES CONTINGENCY ITEM

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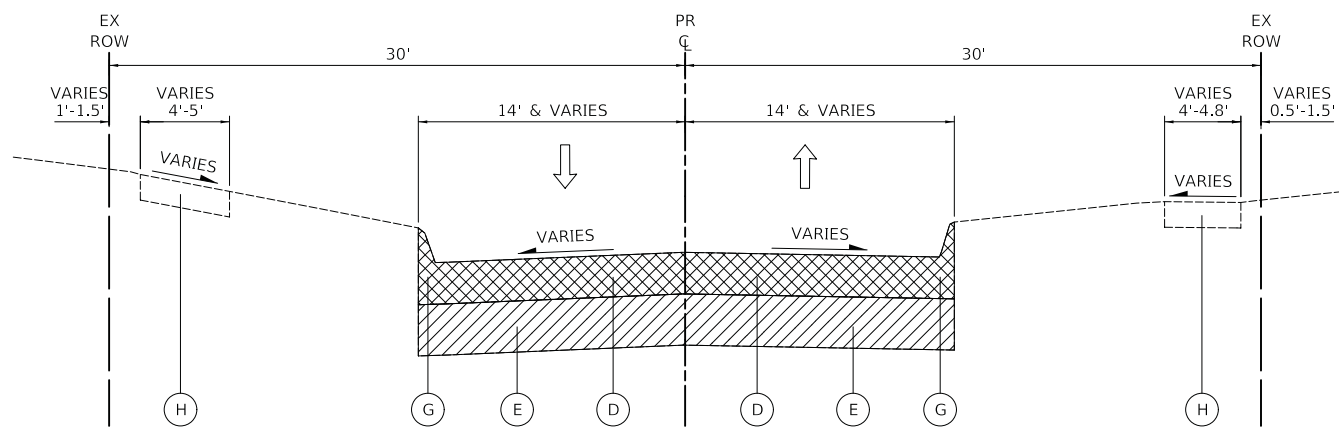
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**VILLAGE OF VILLA PARK**

**2019 STREET IMPROVEMENT PROJECT  
SUMMARY OF QUANTITIES**

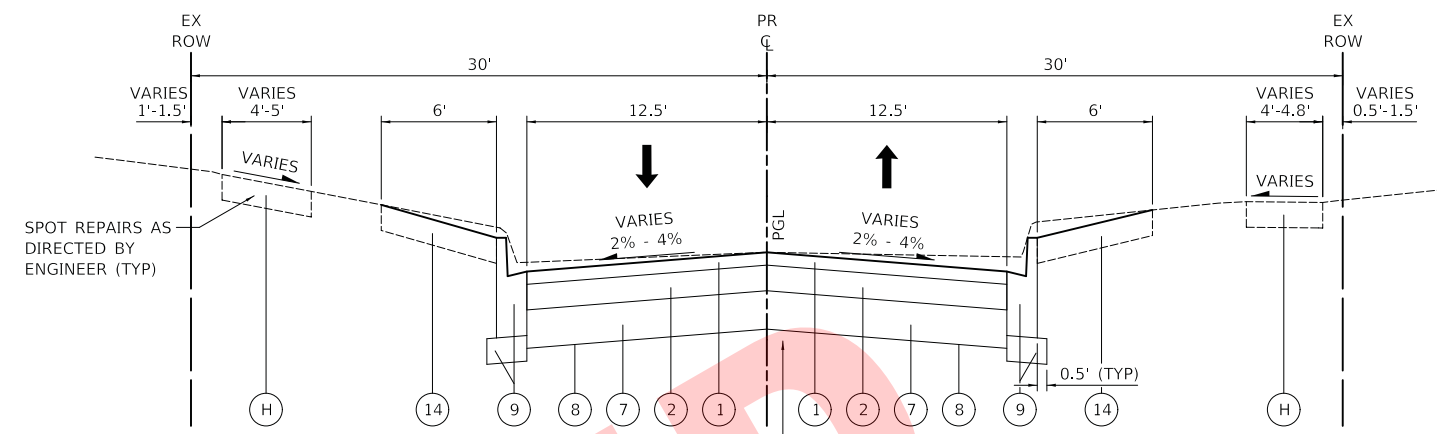
SCALE: N.T.S. SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			37	5
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				



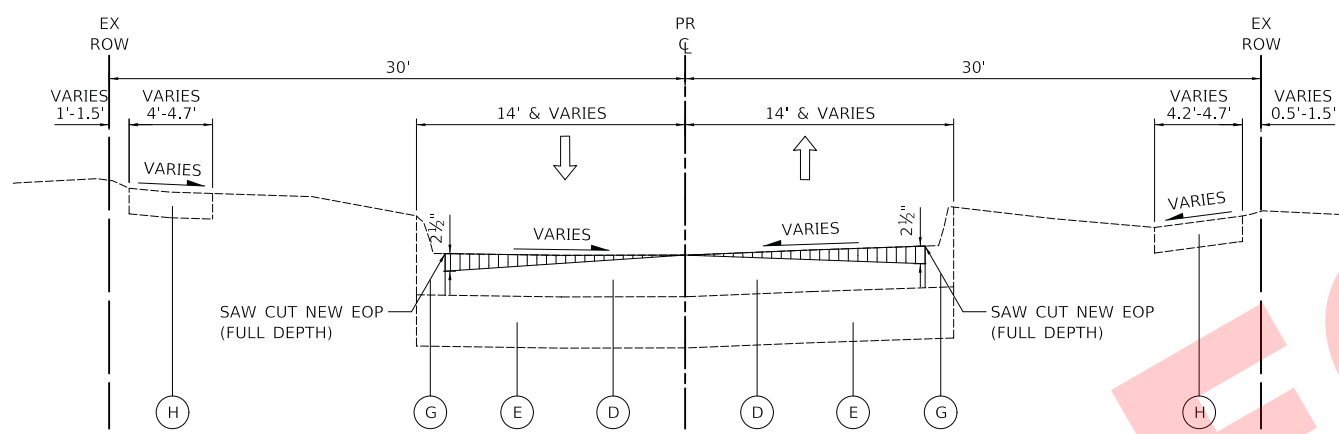
**PLYMOUTH STREET – EXISTING TYPICAL SECTION**

STA. 100+50 TO STA. 103+27  
 STA. 103+95 TO STA. 106+63  
 STA. 110+76 TO STA. 113+50



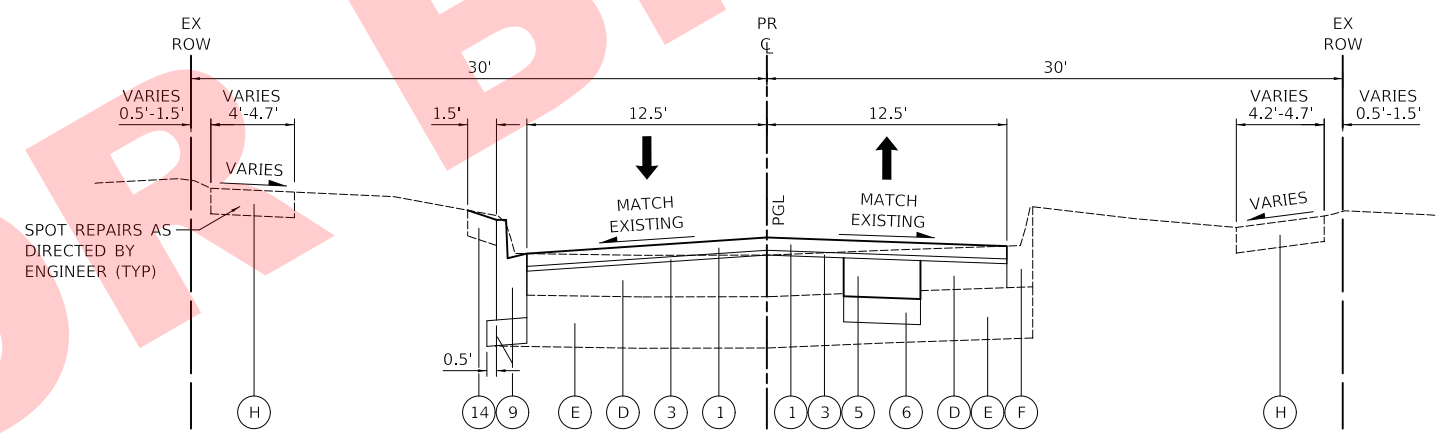
**PLYMOUTH STREET – PROPOSED TYPICAL SECTION**

\*STA. 100+50 TO STA. 103+27  
 \*STA. 103+95 TO STA. 106+63  
 STA. 110+76 TO STA. 113+50  
 \*MAY REQUIRE 8" OF AGGREGATE SUBGRADE IMPROVEMENT



**PLYMOUTH STREET – EXISTING TYPICAL SECTION**

STA. 103+27 TO STA. 103+95  
 STA. 106+63 TO STA. 110+76  
 STA. 113+50 TO STA. 126+44



**PLYMOUTH STREET – PROPOSED TYPICAL SECTION**

STA. 103+27 TO STA. 103+95  
 STA. 106+63 TO STA. 110+76  
 STA. 113+50 TO STA. 126+44

**LEGEND**

- (A) EXISTING HOT-MIX ASPHALT SURFACE COURSE:  
 -PARK BOULEVARD = VARIES 2½" TO 2¾"  
 -ST. CHARLES ALLEY = 2½"  
 -PARK ALLEY = VARIES 0" TO 1"
- (B) EXISTING CRACK CONTROL FABRIC
- (C) EXISTING HOT-MIX ASPHALT BINDER COURSE:  
 -PARK BOULEVARD = 1¾"  
 -ST. CHARLES ALLEY = 3"
- (D) EXISTING PORTLAND CEMENT CONCRETE PAVEMENT:  
 -PLYMOUTH STREET = VARIES 6¼" TO 8¼"  
 -PARK BOULEVARD = VARIES 7¼" TO 9¼"  
 -PARK ALLEY = 7¼"
- (E) EXISTING GRANULAR BASE COURSE:  
 -PLYMOUTH STREET = VARIES 2½" TO 8½"  
 -ST. CHARLES ALLEY = 8½"
- (F) EXISTING COMBINATION CURB AND GUTTER (SEE NOTE 1)
- (G) EXISTING MONOLITHIC BARRIER CURB (SEE NOTE 2)
- (H) EXISTING PORTLAND CEMENT CONCRETE SIDEWALK
- (I) EXISTING HOT-MIX ASPHALT DRIVEWAY PAVEMENT (SEE ROADWAY PLANS FOR EXACT LOCATIONS)
- (J) EXISTING PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT (SEE ROADWAY PLANS FOR EXACT LOCATIONS)
- (1) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2"
- (2) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 4"
- (3) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, ¾"
- (4) PORTLAND CEMENT CONCRETE PAVEMENT 8" (JOINTED)
- (5) CLASS C PATCHES, 6 INCH (SEE NOTE 3)
- (6) AGGREGATE BASE COURSE, TYPE B (SEE NOTE 4)
- (7) AGGREGATE BASE COURSE, TYPE B 6"
- (8) GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- (9) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 (SPECIAL) (AGGREGATE BASE COURSE, TYPE B 4" IS INCLUDED IN COST OF ITEM) (FOR RESURFACING AREAS OF PLYMOUTH STREET, SEE NOTE 3)
- (10) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-4.12 (SPECIAL) (AGGREGATE BASE COURSE, TYPE B 4" IS INCLUDED IN COST OF ITEM) (SEE NOTE 5)
- (11) PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH, SPECIAL (AGGREGATE BASE COURSE, TYPE B 4" IS INCLUDED IN COST OF ITEM)
- (12) HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 6" (AGGREGATE BASE COURSE, TYPE B 8" IS INCLUDED IN COST OF ITEM)
- (13) PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH, SPECIAL (AGGREGATE BASE COURSE, TYPE B 6" IS INCLUDED IN COST OF ITEM)
- (14) TOPSOIL FURNISH AND PLACE, 4" (SPECIAL) SODDING, SPECIAL

- (14) TOPSOIL FURNISH AND PLACE, 4" (SPECIAL) SODDING, SPECIAL
- [Hatched Box] HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH
- [Vertical Lines Box] PORTLAND CEMENT CONCRETE SURFACE REMOVAL (VARIABLE DEPTH)
- [Cross-hatched Box] COMBINATION CURB AND GUTTER REMOVAL (SEE NOTE 1) PAVEMENT REMOVAL (SEE NOTE 2) SIDEWALK REMOVAL & DRIVEWAY PAVEMENT REMOVAL
- [Diagonal Lines Box] EARTH EXCAVATION

**NOTES**

1. THE REMOVAL OF HMA PAVEMENT AND DRIVEWAY PAVEMENT OVERLAID ON CURB AND GUTTER SHALL BE INCLUDED IN THE COST OF "COMBINATION CURB AND GUTTER REMOVAL".
2. THE REMOVAL OF MONOLITHIC BARRIER CURB IN RECONSTRUCTION SECTIONS WILL BE MEASURED FOR PAYMENT IN SQUARE YARDS AND INCLUDED IN THE COST OF "PAVEMENT REMOVAL".
3. THE ENGINEER SHALL MAKE THE FINAL DETERMINATION OF THE LOCATION, WIDTH, AND LENGTH OF CLASS C PATCHES AND CURB AND GUTTER REPLACEMENT IN THE FIELD.
4. DEPENDING ON FIELD CONDITIONS, A MINIMUM OF 4" OF AGGREGATE BASE COURSE, TYPE B WILL BE USED BENEATH PAVEMENT PATCHES AS DIRECTED BY THE ENGINEER.
5. SEE CONSTRUCTION DETAILS FOR CURB AND GUTTER CONSTRUCTION.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
MIXTURE TYPE	AIR VOIDS @ NDES
<b>PAVEMENT RESURFACING</b>	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL 9.5 mm); 2"	4% @ 50 GYR.
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50; 3/4"	3.5% @ 50 GYR.
<b>PAVEMENT RECONSTRUCTION</b>	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL 9.5 mm); 2"	4% @ 50 GYR.
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50; 4"	4% @ 50 GYR.
<b>HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 4"</b>	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL 9.5 mm); 1.5"	4% @ 50 GYR.
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50; 2.5"	4% @ 50 GYR.
<b>HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 6"</b>	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL 9.5 mm); 2"	4% @ 50 GYR.
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50; 4"	4% @ 50 GYR.
<b>CLASS D PATCHES</b>	
CLASS D PATCH (HMA BINDER IL-19.0 mm); 6"	4% @ 70 GYR.

THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT MIXTURES IS 112 LB/SQ YD/IN. THE "AC-TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA MIXES SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.

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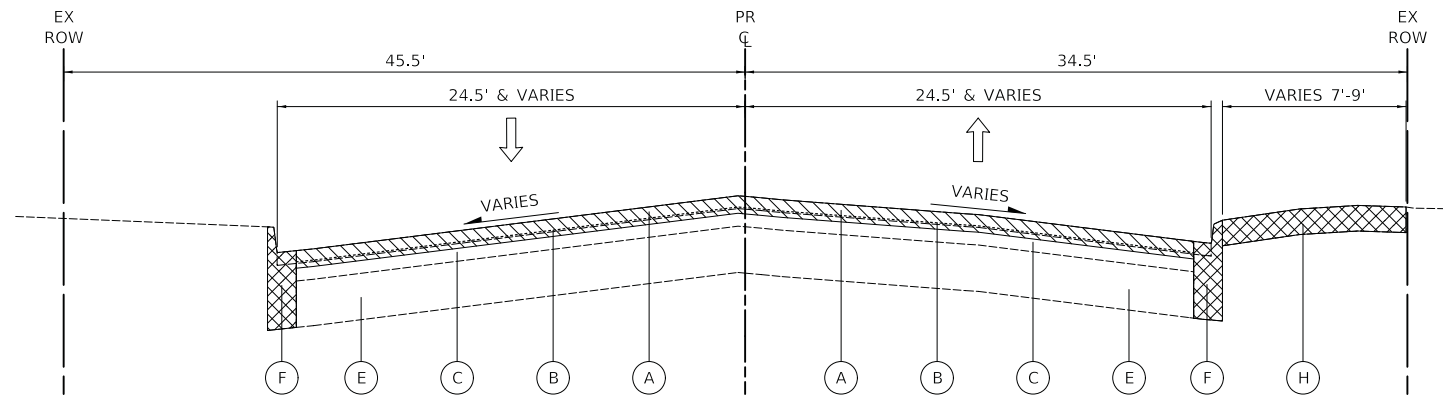


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**VILLAGE OF VILLA PARK**

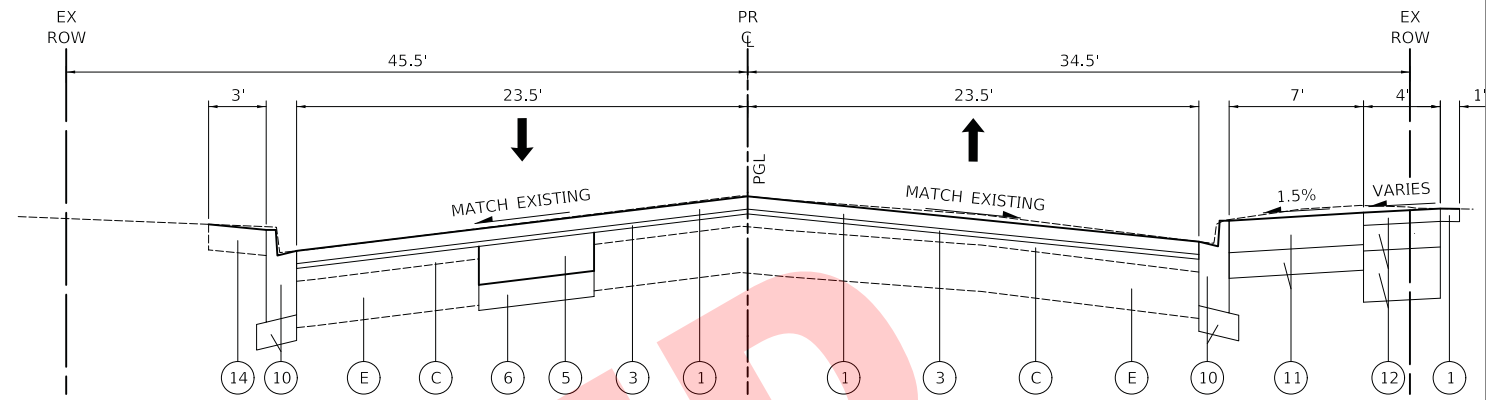
**2019 STREET IMPROVEMENT PROJECT  
 TYPICAL SECTIONS**

SCALE: N.T.S.	SHEET	OF	SHEETS	STA.	TO	STA.	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
									DUPAGE	37	6
										CONTRACT NO.	
										ILLINOIS FED. AID PROJECT	



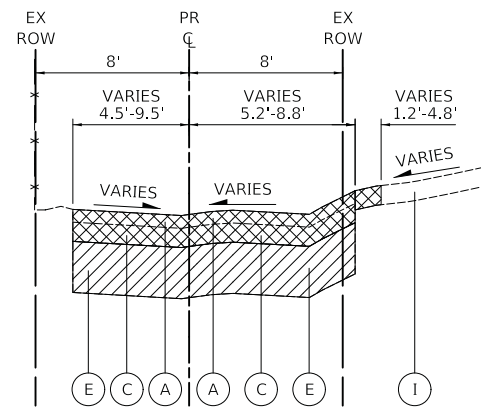
**PARK BOULEVARD – EXISTING TYPICAL SECTION**

STA. 19+66.5 TO STA. 24+37.8



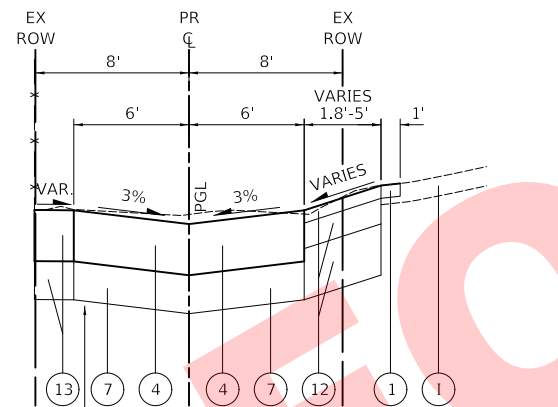
**PARK BOULEVARD – PROPOSED TYPICAL SECTION**

STA. 19+66.5 TO STA. 24+37.8



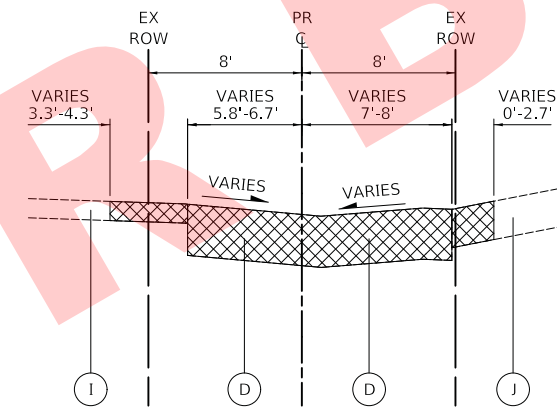
**ST. CHARLES ALLEY – EXISTING TYPICAL SECTION**

STA. 30+11.5 TO STA. 34+45.2



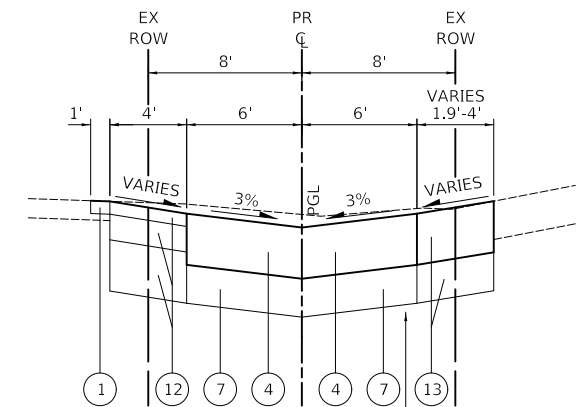
**ST. CHARLES ALLEY – PROPOSED TYPICAL SECTION**

STA. 30+11.5 TO STA. 34+45.2  
MAY REQUIRE 8" OF AGGREGATE SUBGRADE IMPROVEMENT



**PARK ALLEY – EXISTING TYPICAL SECTION**

STA. 10+11.5 TO STA. 14+34.9



**PARK ALLEY – PROPOSED TYPICAL SECTION**

STA. 10+11.5 TO STA. 14+34.9

**LEGEND**

- (A) EXISTING HOT-MIX ASPHALT SURFACE COURSE:  
-PARK BOULEVARD = VARIES 2½" TO 2¾"  
-ST. CHARLES ALLEY = 2½"  
-PARK ALLEY = VARIES 0" TO 1"
- (B) EXISTING CRACK CONTROL FABRIC
- (C) EXISTING HOT-MIX ASPHALT BINDER COURSE:  
-PARK BOULEVARD = 1¾"  
-ST. CHARLES ALLEY = 3"
- (D) EXISTING PORTLAND CEMENT CONCRETE PAVEMENT:  
-PLYMOUTH STREET = VARIES 6¾" TO 8¾"  
-PARK BOULEVARD = VARIES 7¾" TO 9¾"  
-PARK ALLEY = 7¾"
- (E) EXISTING GRANULAR BASE COURSE:  
-PLYMOUTH STREET = VARIES 2½" TO 8½"  
-ST. CHARLES ALLEY = 8½"
- (F) EXISTING COMBINATION CURB AND GUTTER (SEE NOTE 1)
- (G) EXISTING MONOLITHIC BARRIER CURB (SEE NOTE 2)
- (H) EXISTING PORTLAND CEMENT CONCRETE SIDEWALK
- (I) EXISTING HOT-MIX ASPHALT DRIVEWAY PAVEMENT (SEE ROADWAY PLANS FOR EXACT LOCATIONS)
- (J) EXISTING PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT (SEE ROADWAY PLANS FOR EXACT LOCATIONS)
- (1) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2"
- (2) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 4"
- (3) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, ¾"
- (4) PORTLAND CEMENT CONCRETE PAVEMENT 8" (JOINTED)
- (5) CLASS C PATCHES, 6 INCH (SEE NOTE 3)
- (6) AGGREGATE BASE COURSE, TYPE B (SEE NOTE 4)
- (7) AGGREGATE BASE COURSE, TYPE B 6"
- (8) GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- (9) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 (SPECIAL) (AGGREGATE BASE COURSE, TYPE B 4" IS INCLUDED IN COST OF ITEM) (FOR RESURFACING AREAS OF PLYMOUTH STREET, SEE NOTE 3)
- (10) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-4.12 (SPECIAL) (AGGREGATE BASE COURSE, TYPE B 4" IS INCLUDED IN COST OF ITEM) (SEE NOTE 5)
- (11) PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH, SPECIAL (AGGREGATE BASE COURSE, TYPE B 4" IS INCLUDED IN COST OF ITEM)
- (12) HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 6" (AGGREGATE BASE COURSE, TYPE B 8" IS INCLUDED IN COST OF ITEM)
- (13) PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH, SPECIAL (AGGREGATE BASE COURSE, TYPE B 6" IS INCLUDED IN COST OF ITEM)
- (14) TOPSOIL FURNISH AND PLACE, 4" (SPECIAL) SODDING, SPECIAL

- HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH
- PORTLAND CEMENT CONCRETE SURFACE REMOVAL (VARIABLE DEPTH)
- COMBINATION CURB AND GUTTER REMOVAL (SEE NOTE 1) PAVEMENT REMOVAL (SEE NOTE 2) SIDEWALK REMOVAL & DRIVEWAY PAVEMENT REMOVAL
- EARTH EXCAVATION

**NOTES**

1. THE REMOVAL OF HMA PAVEMENT AND DRIVEWAY PAVEMENT OVERLAID ON CURB AND GUTTER SHALL BE INCLUDED IN THE COST OF "COMBINATION CURB AND GUTTER REMOVAL".
2. THE REMOVAL OF MONOLITHIC BARRIER CURB IN RECONSTRUCTION SECTIONS WILL BE MEASURED FOR PAYMENT IN SQUARE YARDS AND INCLUDED IN THE COST OF "PAVEMENT REMOVAL".
3. THE ENGINEER SHALL MAKE THE FINAL DETERMINATION OF THE LOCATION, WIDTH, AND LENGTH OF CLASS C PATCHES AND CURB AND GUTTER REPLACEMENT IN THE FIELD.
4. DEPENDING ON FIELD CONDITIONS, A MINIMUM OF 4" OF AGGREGATE BASE COURSE, TYPE B WILL BE USED BENEATH PAVEMENT PATCHES AS DIRECTED BY THE ENGINEER.
5. SEE CONSTRUCTION DETAILS FOR CURB AND GUTTER CONSTRUCTION.

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PLOT DATE = 6/21/2019	CHECKED - JRV	REVISED -
	DATE - 06/21/19	REVISED -

VILLAGE OF VILLA PARK

2019 STREET IMPROVEMENT PROJECT  
TYPICAL SECTIONS

SCALE: N.T.S. SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DUPAGE	37	7
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

### EARTHWORK SCHEDULES

PLYMOUTH STREET - EARTHWORK SCHEDULE					
STATION	DISTANCE	EARTH EXCAVATION	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	EARTH EXCAVATION	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL
	(FT)	(SQ FT)	(SQ FT)	(CU YD)	(CU YD)
100+50		0.0	33.2		
	25			0.0	30.4
100+75		0.0	32.3		
	25			0.0	29.2
101+00		0.0	30.7		
	50			0.0	56.3
101+50		0.0	30.0		
	50			0.0	56.4
102+00		0.0	30.8		
	40			0.0	47.1
102+40		0.0	32.8		
	60			0.0	73.1
103+00		0.0	33.0		
	27			0.0	31.8
103+27		0.0	30.6		
103+95		0.0	30.8		
	30			0.0	35.2
104+25		0.0	32.7		
	65			0.0	77.9
104+90		0.0	32.1		
	40			0.0	45.3
105+30		0.0	29.1		
	70			0.0	73.8
106+00		0.0	27.8		
	30			0.0	29.7
106+30		0.0	25.6		
	33			0.0	33.1
106+63		0.0	28.6		
110+76		10.3	0.0		
	49			18.9	0.0
111+25		10.5	0.0		
	50			19.8	0.0
111+75		10.9	0.0		
	50			20.7	0.0
112+25		11.5	0.0		
	50			20.1	0.0
112+75		10.2	0.0		
	50			17.6	0.0
113+25		8.8	0.0		
	25			8.5	0.0
113+50		9.5	0.0		
SUB-TOTAL =				105.7	619.4
PLYMOUTH STREET TOTAL =				105.7	619.4

ST CHARLES ALLEY - EARTHWORK SCHEDULE					
STATION	DISTANCE	EARTH EXCAVATION	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	EARTH EXCAVATION	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL
	(FT)	(SQ FT)	(SQ FT)	(CU YD)	(CU YD)
30+30		0.0	30.3		
	7.5			0.0	8.4
30+38		0.0	30.5		
	12.5			0.0	14.5
30+50		0.0	32.0		
	25			0.0	29.4
30+75		0.0	31.5		
	25			0.0	28.5
31+00		0.0	30.1		
	25			0.0	28.0
31+25		0.0	30.4		
	25			0.0	27.9
31+50		0.0	29.9		
	25			0.0	30.3
31+75		0.0	35.5		
	25			0.0	32.9
32+00		0.0	35.6		
	12.5			0.0	15.4
32+12.50		0.0	31.0		
	12.5			0.0	14.3
32+25		0.0	30.7		
	12.5			0.0	14.5
32+37.50		0.0	32.1		
	15.5			0.0	18.9
32+53		0.0	33.8		
	22			0.0	28.2
32+75		0.0	35.3		
	25			0.0	31.4
33+00		0.0	32.5		
	12.5			0.0	13.1
33+12.50		0.0	23.9		
	12.5			0.0	11.5
33+25		0.0	25.7		
	21			0.0	19.7
33+46		0.0	25.1		
	14			0.0	13.4
33+60		0.0	26.5		
	10			0.0	9.7
33+70		0.0	26.1		
	17.5			0.0	18.0
33+87.50		0.0	29.3		
	12.5			0.0	13.6
34+00		0.0	29.6		
	12.5			0.0	13.6
34+12.50		0.0	29.4		
	13.74			0.0	17.7
34+26.24		0.0	40.3		
SUB-TOTAL =				0.0	453.0
ST CHARLES ALLEY TOTAL =				0.0	453.0

PARK ALLEY - EARTHWORK SCHEDULE					
STATION	DISTANCE	EARTH EXCAVATION	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	EARTH EXCAVATION	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL
	(FT)	(SQ FT)	(SQ FT)	(CU YD)	(CU YD)
10+31.53		0.0	11.1		
	5.97			0.0	2.4
10+37.50		0.0	10.6		
	10			0.0	4.2
10+47.50		0.0	12.2		
	5			0.0	2.5
10+52.50		0.0	14.4		
	22.5			0.0	12.1
10+75		0.0	14.5		
	25			0.0	13.5
11+00		0.0	14.6		
	35			0.0	18.9
11+35		0.0	14.7		
	9.5			0.0	4.7
11+44.50		0.0	12.3		
	5.5			0.0	2.7
11+50		0.0	14.6		
	25			0.0	13.5
11+75		0.0	14.4		
	25			0.0	13.0
12+00		0.0	13.7		
	20			0.0	10.5
12+20		0.0	14.7		
	30			0.0	17.2
12+50		0.0	16.2		
	25			0.0	15.4
12+75		0.0	17.1		
	25			0.0	14.5
13+00		0.0	14.1		
	25			0.0	13.6
13+25		0.0	15.3		
	6			0.0	3.5
13+31		0.0	16.6		
	4.75			0.0	2.8
13+35.75		0.0	15.1		
	14.25			0.0	7.8
13+50		0.0	14.5		
	25			0.0	12.3
13+75		0.0	12.1		
	25			0.0	14.5
14+00		0.0	19.1		
	24.16			0.0	15.6
14+24.16		0.0	15.7		
SUB-TOTAL =				0.0	215.2
PARK ALLEY TOTAL =				0.0	215.2

#### NOTES

1. THE PLYMOUTH STREET EARTHWORK SCHEDULE INCLUDES A PRESUMED UNDERCUT OF 8" IN ITS CALCULATIONS FROM STATION 100+50 TO 106+63.
2. THE ST. CHARLES ALLEY EARTHWORK SCHEDULE INCLUDES A PRESUMED UNDERCUT OF 8" IN ITS CALCULATIONS FOR THE ENTIRE ALLEY LENGTH.

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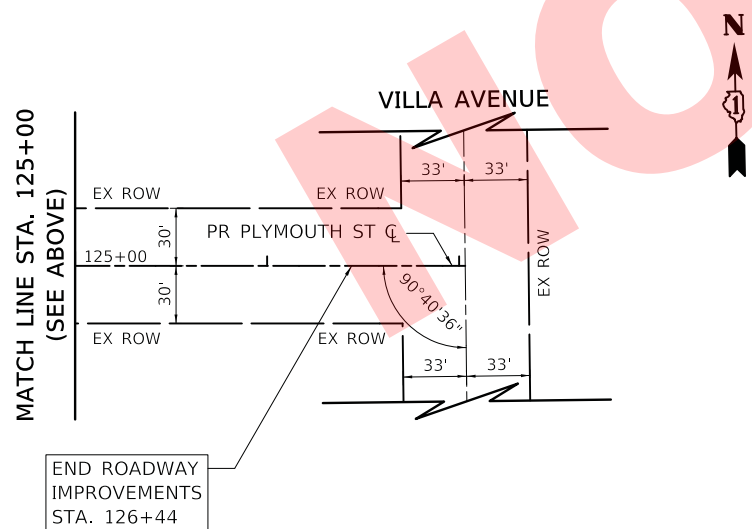
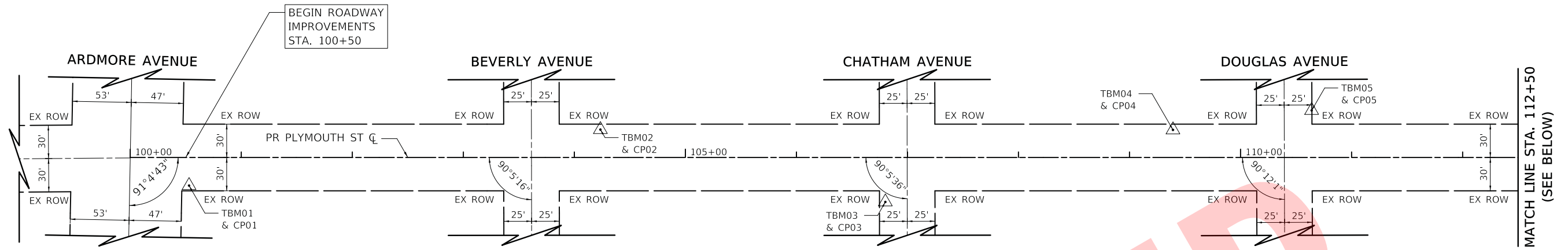
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**VILLAGE OF VILLA PARK**

**2019 STREET IMPROVEMENT PROJECT  
EARTHWORK SCHEDULES**

SCALE: N.T.S. SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			37	8
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				



PROPOSED PLYMOUTH STREET CENTERLINE			
DESCRIPTION	STATION	NORTHING	EASTING
B.O.A.	100+00.00	1,906,187.7321	1,080,741.5780
E.O.A.	127+03.19	1,906,126.1714	1,083,444.0645

**BASIS OF COORDINATES**

COORDINATES ARE BASED ON THE ILLINOIS STATE PLANE COORDINATE SYSTEM (NAD 83).

**BASIS OF ELEVATIONS**

DUPAGE COUNTY BENCHMARK NO. YK03001, PID: DK 3101 STATION IS AT THE SOUTHWEST CORNER OF THE INTERSECTION OF NORTH AVENUE (ILLINOIS ROUTE 64) AND VILLA AVENUE. STATION IS 58.6 FEET SOUTH OF THE CENTERLINE OF NORTH AVENUE, 59.3 FEET WEST OF THE CENTERLINE OF VILLA AVENUE, AND 26.5 FEET NORTHWEST OF A FIRE HYDRANT. MONUMENT IS 1.2 FEET ABOVE ROAD SURFACE ON A 2.5 FOOT DIAMETER CONCRETE TRAFFIC SIGNAL BASE AND IS FERRO MAGNETIC. ELEVATION 679.61 N.A.V.D. 1988 DATUM.

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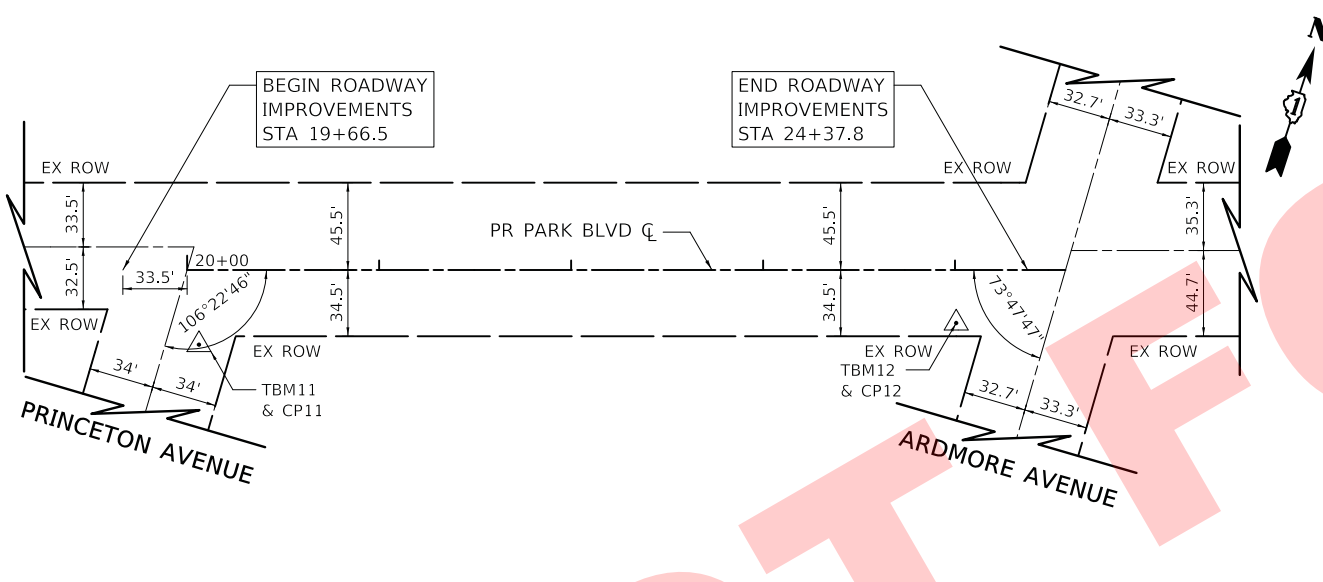
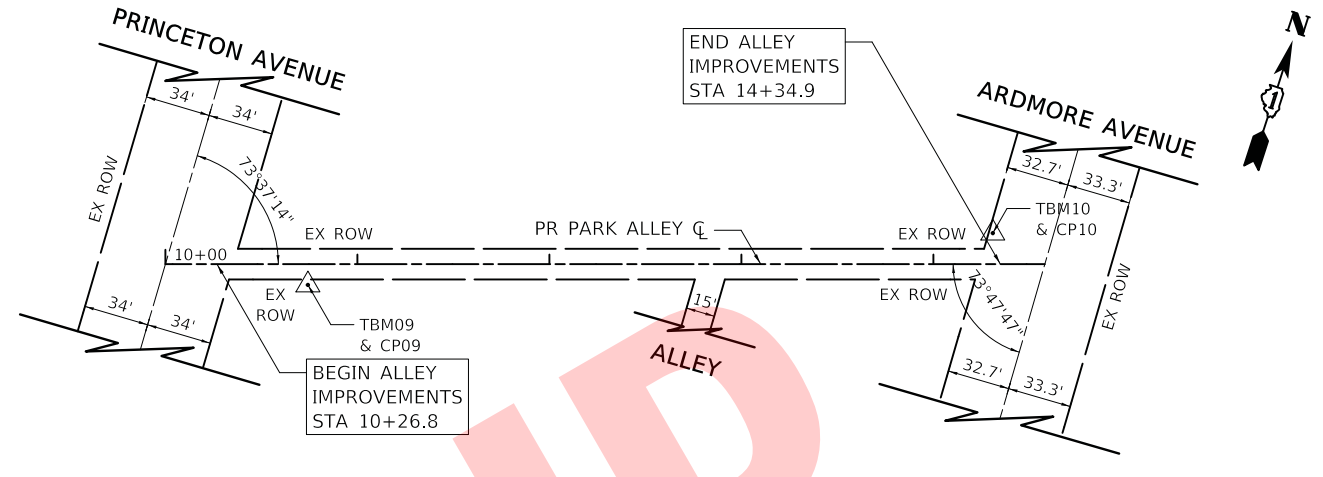
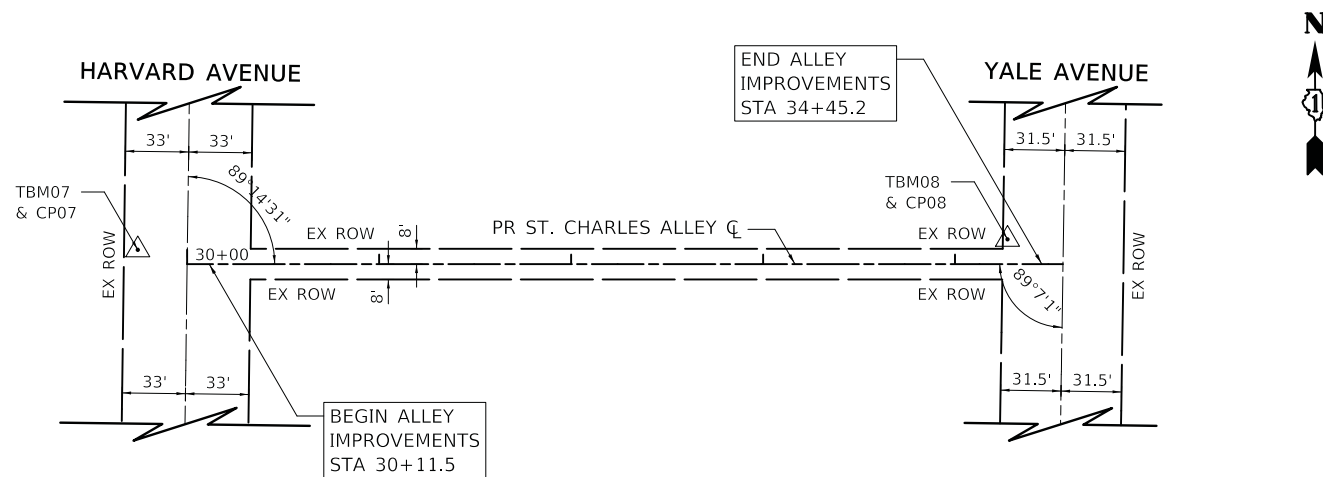
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	DATE - 06/21/19	REVISED -

**VILLAGE OF VILLA PARK**

**2019 STREET IMPROVEMENT PROJECT  
ALIGNMENT, TIES & BENCHMARKS**

SCALE: 1" = 50' SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DUPAGE	37	9
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				



BENCHMARK AND CONTROL POINT INFORMATION						
BM / CP	STATION	OFFSET	NORTHING	EASTING	ELEVATION	DESCRIPTION
TBM01 CP01	100+52.96	25.30' RT	1,906,161.2320	1,080,793.9500	690.22	CROSS CUT (SET) IN SIDEWALK LOCATED AT SOUTHEAST QUADRANT OF THE INTERSECTION OF ARDMORE AVENUE AND PLYMOUTH STREET
TBM02 CP02	104+23.47	25.10' LT	1,906,203.1790	1,081,165.5080	691.64	CROSS CUT (SET) IN SIDEWALK LOCATED ON NORTH SIDE OF PLYMOUTH STREET IN FRONT OF PROPERTY 30 EAST ARDMORE AVENUE
TBM03 CP03	106+80.11	40.05' RT	1,906,132.2080	1,081,420.5960	689.31	CROSS CUT (SET) IN SIDEWALK LOCATED AT SOUTHWEST QUADRANT OF THE INTERSECTION OF PLYMOUTH STREET AND CHATHAM AVENUE
TBM04 CP04	109+39.12	24.82' LT	1,906,191.1620	1,081,681.0220	691	CROSS CUT (SET) IN SIDEWALK LOCATED ON NORTH SIDE OF PLYMOUTH STREET IN FRONT OF PROPERTY 502 NORTH DOUGLAS AVENUE
TBM05 CP05	110+64.06	42.33' LT	1,906,205.8230	1,081,806.3230	689.52	CROSS CUT (SET) IN SIDEWALK LOCATED AT NORTHEAST QUADRANT OF THE INTERSECTION OF PLYMOUTH STREET AND DOUGLAS AVENUE
TBM06 CP06	113+33.63	24.83' LT	1,906,182.1800	1,082,075.4300	687.24	CROSS CUT (SET) IN SIDEWALK LOCATED AT NORTHWEST QUADRANT OF THE INTERSECTION OF PLYMOUTH STREET AND ELLSWORTH AVENUE
TBM07 CP07	29+74.29	7.44' LT	1902800.901	1079331.416	695.6	CROSS CUT (SET) IN SIDEWALK LOCATED ON WEST SIDE OF THE INTERSECTION OF HARVARD AVENUE AND ALLEY
TBM08 CP08	34+27.32	12.73' LT	1902805.514	1079784.455	691.63	CROSS CUT (SET) IN SIDEWALK LOCATED AT NORTHWEST QUADRANT OF THE INTERSECTION OF YALE AVENUE AND ALLEY
TBM09 CP09	10+74.45	9.93' RT	1899976.097	1080392.931	703.94	MAGNETIC NAIL (SET) IN PARKING LOT LOCATED ON SOUTH SIDE OF ALLEY AT PROPERTY 26 WEST HIGHLAND AVENUE
TBM10 CP10	14+31.24	17.03' LT	1900111.791	1080724.006	700.32	CROSS CUT (SET) IN SIDEWALK LOCATED AT NORTHWEST QUADRANT OF THE INTERSECTION OF ARDMORE AVENUE AND ALLEY
TBM11 CP11	20+06.14	38.91' RT	1900096.114	1080332.213	703.2	CROSS CUT (SET) IN SIDEWALK LOCATED AT SOUTHEAST QUADRANT OF THE INTERSECTION OF PARK BOULEVARD AND PRINCETON AVENUE
TBM12 CP12	24+00.61	27.61' RT	1900228.532	1080703.972	699.23	CROSS CUT (SET) IN SIDEWALK LOCATED AT SOUTHWEST QUADRANT OF THE INTERSECTION OF PARK BOULEVARD AND ARDMORE AVENUE

PROPOSED ST. CHARLES ALLEY CENTERLINE			
DESCRIPTION	STATION	NORTHING	EASTING
B.O.A.	30+00.00	1,902,793.5701	1,079,357.1158
E.O.A.	34+56.24	1,902,792.7319	1,079,813.3558

PROPOSED PARK ALLEY CENTERLINE			
DESCRIPTION	STATION	NORTHING	EASTING
B.O.A.	10+00.00	1,899,962.5788	1,080,319.0428
E.O.A.	14+58.09	1,900,103.8672	1,080,754.7991

PROPOSED PARK BOULEVARD CENTERLINE			
DESCRIPTION	STATION	NORTHING	EASTING
B.O.A.	20+00.00	1,900,131.2343	1,080,314.3753
E.O.A.	24+57.55	1,900,272.3564	1,080,749.6188

**BASIS OF COORDINATES**

COORDINATES ARE BASED ON THE ILLINOIS STATE PLANE COORDINATE SYSTEM (NAD 83).

**BASIS OF ELEVATIONS**

DUPAGE COUNTY BENCHMARK NO. YK03001, PID: DK 3101 STATION IS AT THE SOUTHWEST CORNER OF THE INTERSECTION OF NORTH AVENUE (ILLINOIS ROUTE 64) AND VILLA AVENUE. STATION IS 58.6 FEET SOUTH OF THE CENTERLINE OF NORTH AVENUE, 59.3 FEET WEST OF THE CENTERLINE OF VILLA AVENUE, AND 26.5 FEET NORTHWEST OF A FIRE HYDRANT. MONUMENT IS 1.2 FEET ABOVE ROAD SURFACE ON A 2.5 FOOT DIAMETER CONCRETE TRAFFIC SIGNAL BASE AND IS FERRO MAGNETIC. ELEVATION 679.61 N.A.V.D. 1988 DATUM.

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**VILLAGE OF VILLA PARK**

**2019 STREET IMPROVEMENT PROJECT  
ALIGNMENT, TIES & BENCHMARKS**

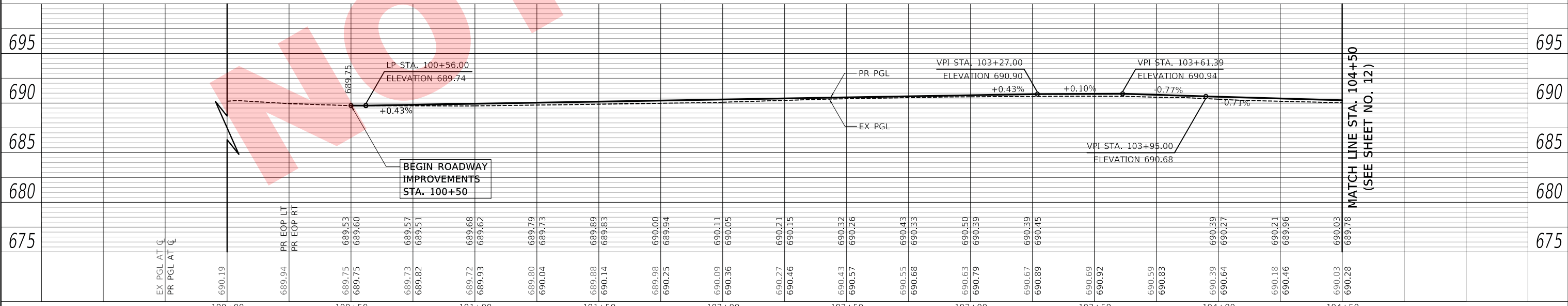
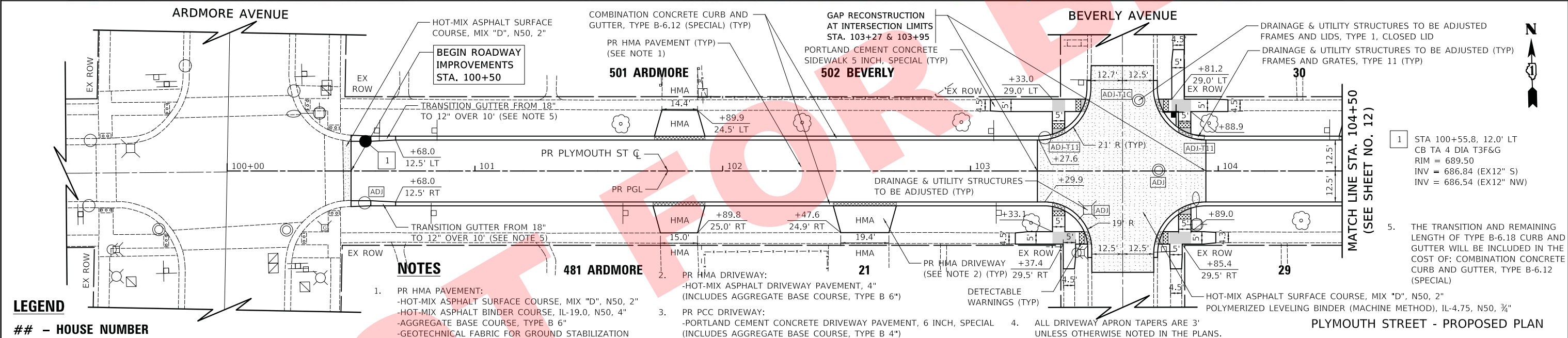
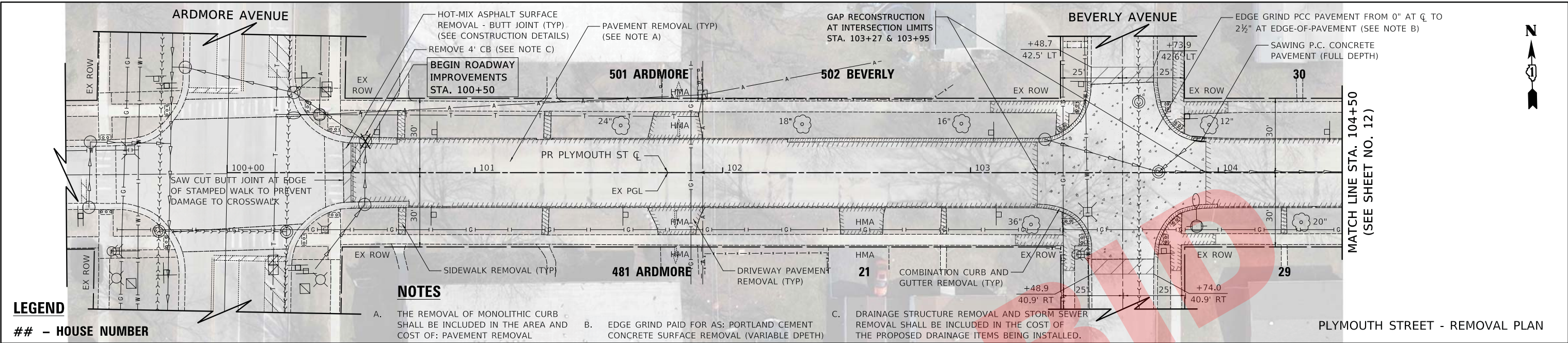
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DUPAGE	37	10
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

DATE	
BY	
PLAN	
NO.	
NO.	
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DATE	
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PROFILE	
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NO.	

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690	690	690	690	690	690	690	690	690	690
685	685	685	685	685	685	685	685	685	685
680	680	680	680	680	680	680	680	680	680
675	675	675	675	675	675	675	675	675	675



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VILLAGE OF VILLA PARK

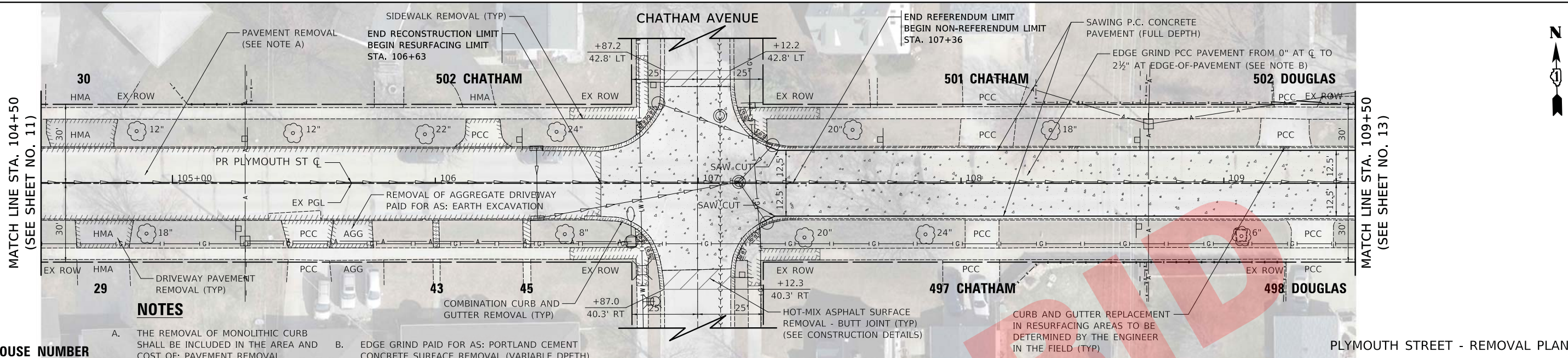
PLYMOUTH STREET	
ROADWAY REMOVAL, PROPOSED PLAN AND PROFILE	
SCALE: 1" = 20'	SHEET OF SHEETS
STA. 100+50	TO STA. 104+50

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DUPAGE	37	11
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

DATE	
BY	
PLAN	
NO.	
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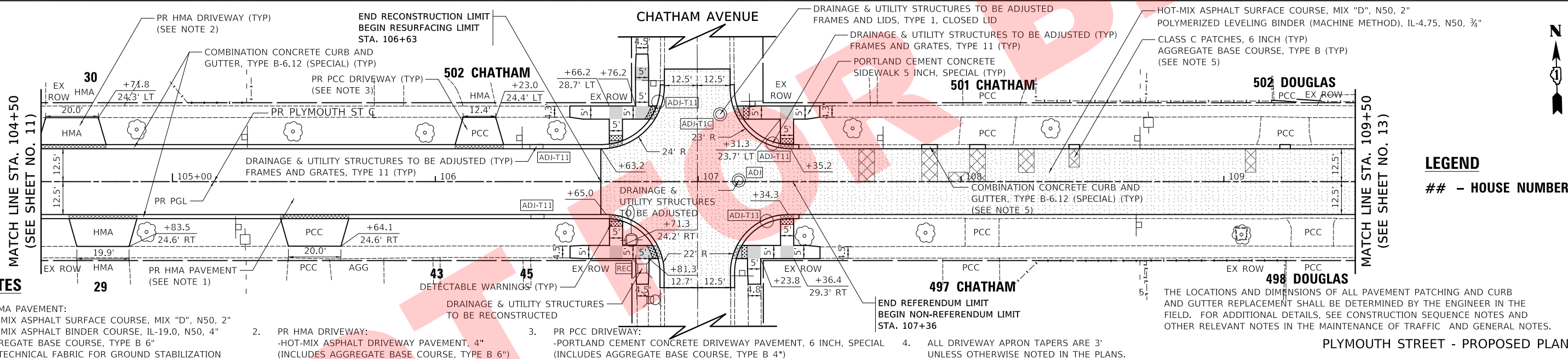


**LEGEND**

## - HOUSE NUMBER

**NOTES**

- A. THE REMOVAL OF MONOLITHIC CURB SHALL BE INCLUDED IN THE AREA AND COST OF: PAVEMENT REMOVAL
- B. EDGE GRIND PAID FOR AS: PORTLAND CEMENT CONCRETE SURFACE REMOVAL (VARIABLE DPETH)

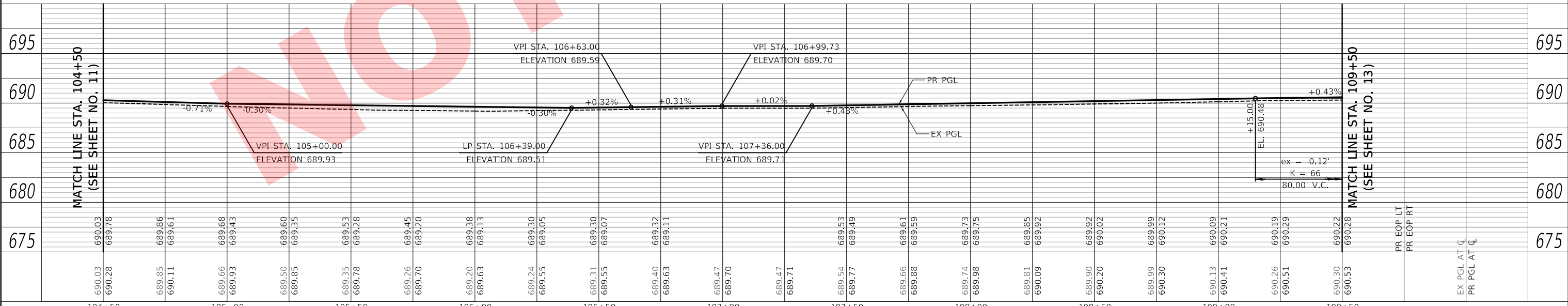


**NOTES**

- 1. PR HMA PAVEMENT:  
 -HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2"  
 -HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 4"  
 -AGGREGATE BASE COURSE, TYPE B 6"  
 -GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- 2. PR HMA DRIVEWAY:  
 -HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 4"  
 (INCLUDES AGGREGATE BASE COURSE, TYPE B 6")
- 3. PR PCC DRIVEWAY:  
 -PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH, SPECIAL  
 (INCLUDES AGGREGATE BASE COURSE, TYPE B 4")
- 4. ALL DRIVEWAY APRON TAPERS ARE 3' UNLESS OTHERWISE NOTED IN THE PLANS.

**LEGEND**

## - HOUSE NUMBER



104+50	105+00	105+50	106+00	106+50	107+00	107+50	108+00	108+50	109+00	109+50
690.03	690.28	689.85	689.63	689.24	689.31	689.47	689.54	689.66	690.13	690.22
689.78	689.86	689.61	689.35	689.53	689.78	689.70	689.77	689.88	690.09	690.28
689.86	689.61	689.68	689.35	689.28	689.45	689.71	689.75	689.81	690.09	690.28
689.61	689.68	689.43	689.50	689.35	689.26	689.47	689.74	689.81	690.13	690.28
689.68	689.43	689.60	689.35	689.28	689.45	689.71	689.75	689.81	690.09	690.28
689.43	689.60	689.35	689.28	689.20	689.70	689.71	689.75	689.81	690.09	690.28
689.60	689.35	689.28	689.20	689.30	689.70	689.71	689.75	689.81	690.09	690.28
689.35	689.28	689.20	689.30	689.07	689.70	689.71	689.75	689.81	690.09	690.28
689.28	689.20	689.30	689.07	689.32	689.70	689.71	689.75	689.81	690.09	690.28
689.20	689.30	689.07	689.32	689.11	689.70	689.71	689.75	689.81	690.09	690.28
689.30	689.07	689.32	689.11	689.47	689.70	689.71	689.75	689.81	690.09	690.28
689.07	689.32	689.11	689.47	689.70	689.71	689.75	689.81	690.09	690.28	690.28
689.32	689.11	689.47	689.70	689.71	689.75	689.81	690.09	690.28	690.28	690.28
689.11	689.47	689.70	689.71	689.75	689.81	690.09	690.28	690.28	690.28	690.28
689.47	689.70	689.71	689.75	689.81	690.09	690.28	690.28	690.28	690.28	690.28
689.70	689.71	689.75	689.81	690.09	690.28	690.28	690.28	690.28	690.28	690.28
689.71	689.75	689.81	690.09	690.28	690.28	690.28	690.28	690.28	690.28	690.28
689.75	689.81	690.09	690.28	690.28	690.28	690.28	690.28	690.28	690.28	690.28
689.81	690.09	690.28	690.28	690.28	690.28	690.28	690.28	690.28	690.28	690.28
690.09	690.28	690.28	690.28	690.28	690.28	690.28	690.28	690.28	690.28	690.28
690.28	690.28	690.28	690.28	690.28	690.28	690.28	690.28	690.28	690.28	690.28
690.28	690.28	690.28	690.28	690.28	690.28	690.28	690.28	690.28	690.28	690.28
690.28	690.28	690.28	690.28	690.28	690.28	690.28	690.28	690.28	690.28	690.28



USER NAME = mjp	DESIGNED - MJP	REVISED -
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	CHECKED - JRV	REVISED -
	DATE - 06/21/19	REVISED -

VILLAGE OF VILLA PARK

PLYMOUTH STREET  
ROADWAY REMOVAL, PROPOSED PLAN AND PROFILE

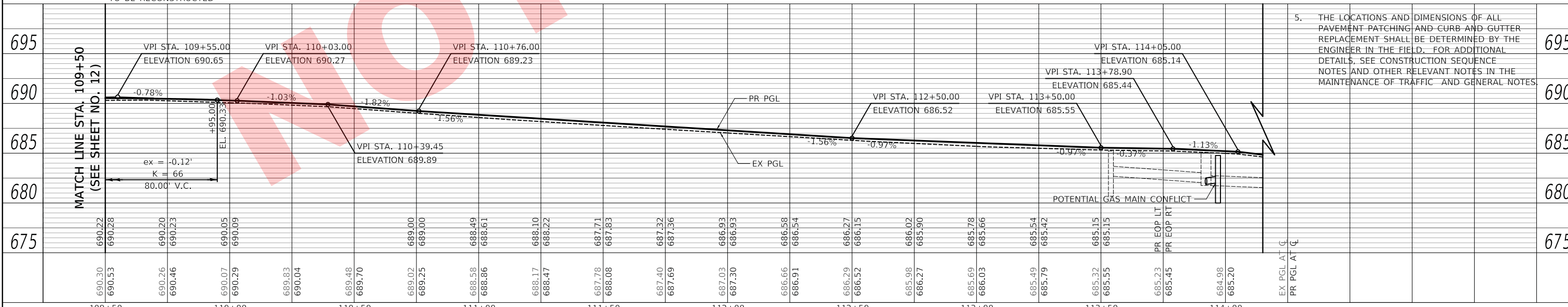
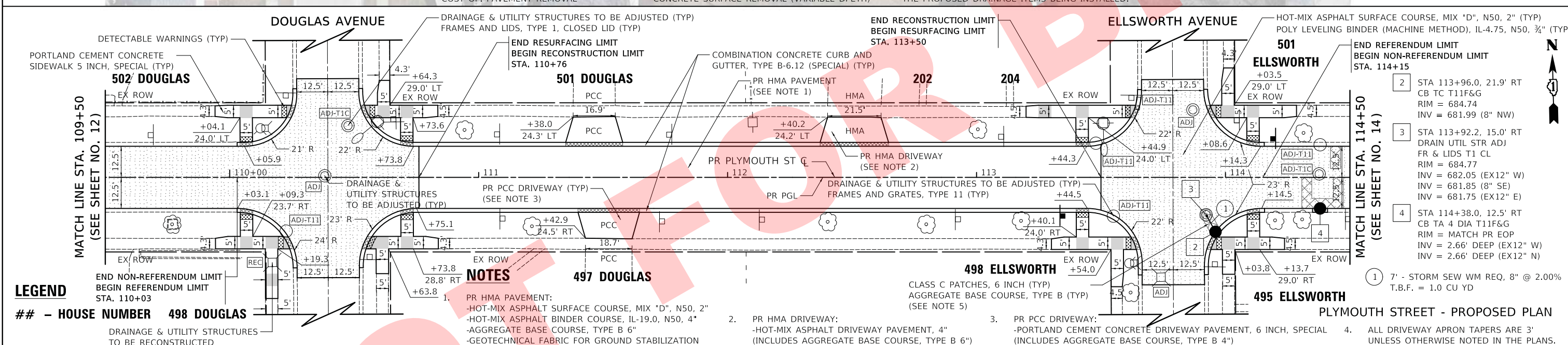
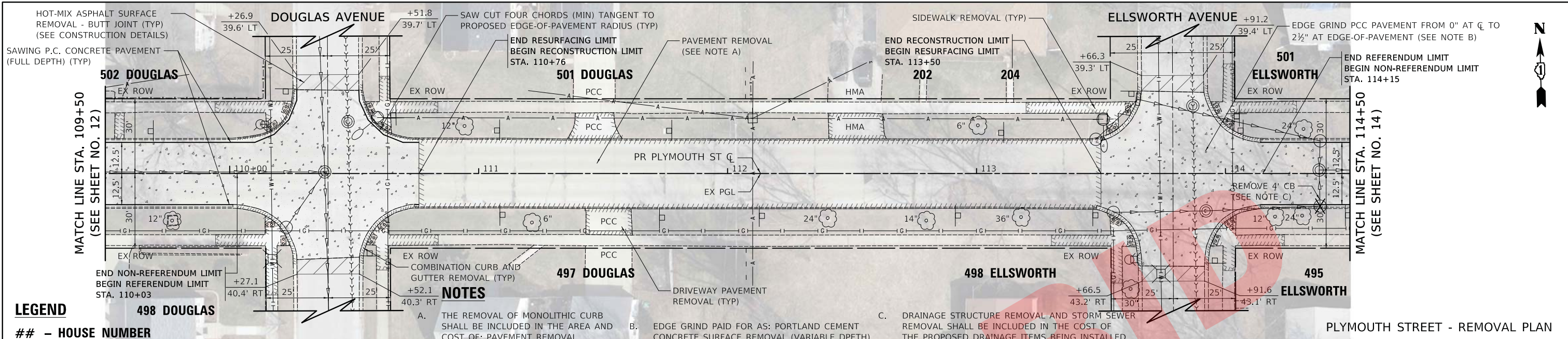
SCALE: 1" = 20' SHEET OF SHEETS STA. 104+50 TO STA. 109+50

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			37	12
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

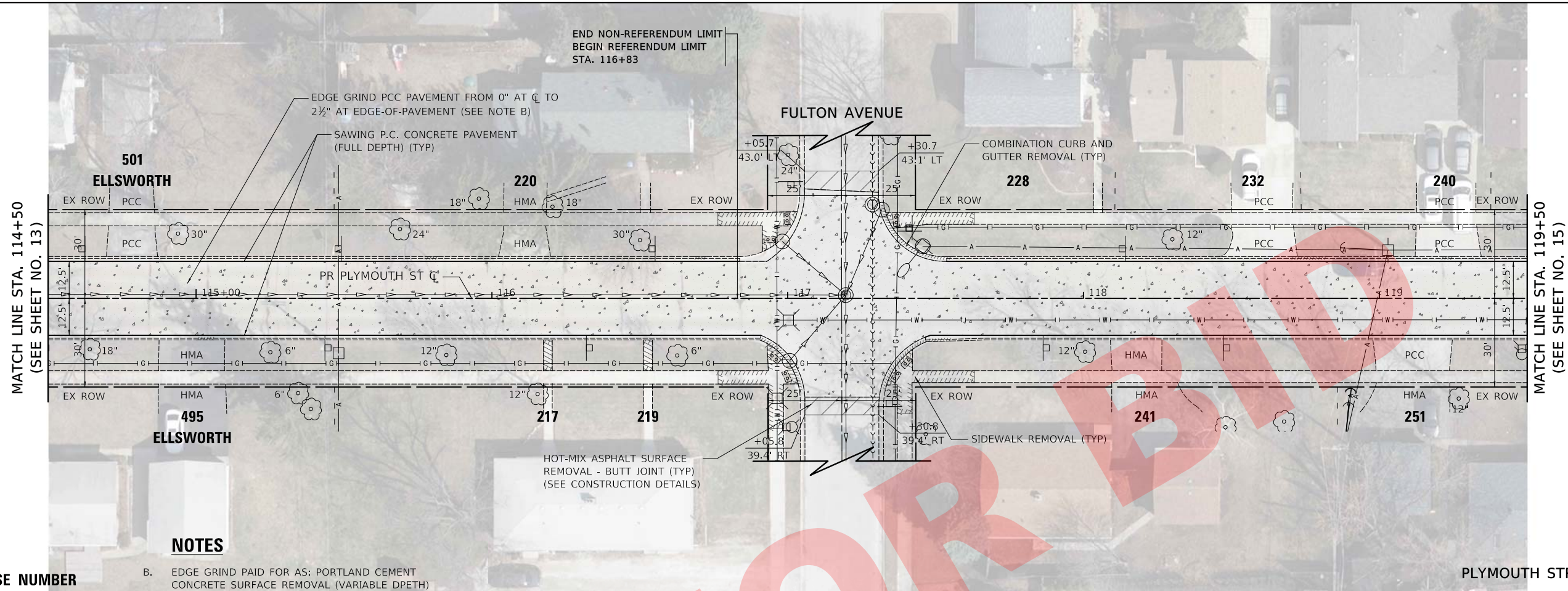
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	PLOT SCALE = 20.0000' / in.	CHECKED - JRV	REVISED -					DUPAGE	37	13		
	PLOT DATE = 6/21/2019	DATE - 06/21/19	REVISED -	SCALE: 1" = 20'		SHEET OF SHEETS STA. 109+50 TO STA. 114+50		ILLINOIS FED. AID PROJECT				

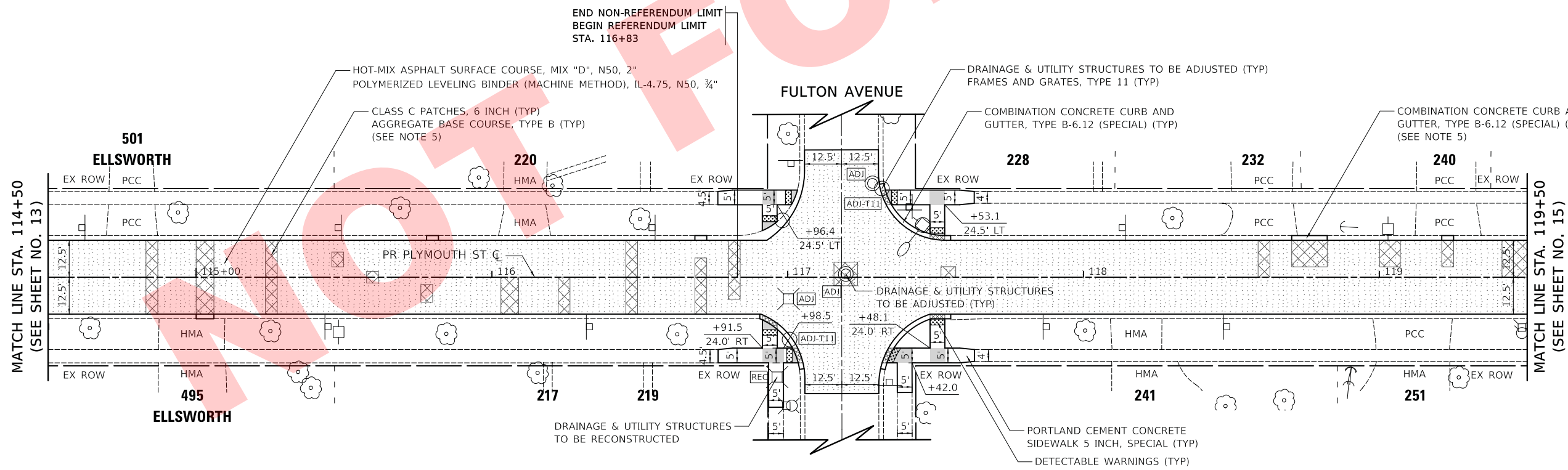


**LEGEND**

## - HOUSE NUMBER

**NOTES**

- B. EDGE GRIND PAID FOR AS: PORTLAND CEMENT CONCRETE SURFACE REMOVAL (VARIABLE DPETH)



**LEGEND**

## - HOUSE NUMBER

**NOTES**

- 5. THE LOCATIONS AND DIMENSIONS OF ALL PAVEMENT PATCHING AND CURB AND GUTTER REPLACEMENT SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD. FOR ADDITIONAL DETAILS, SEE CONSTRUCTION SEQUENCE NOTES AND OTHER RELEVANT NOTES IN THE MAINTENANCE OF TRAFFIC AND GENERAL NOTES.

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	DATE - 06/21/19	REVISED -

VILLAGE OF VILLA PARK

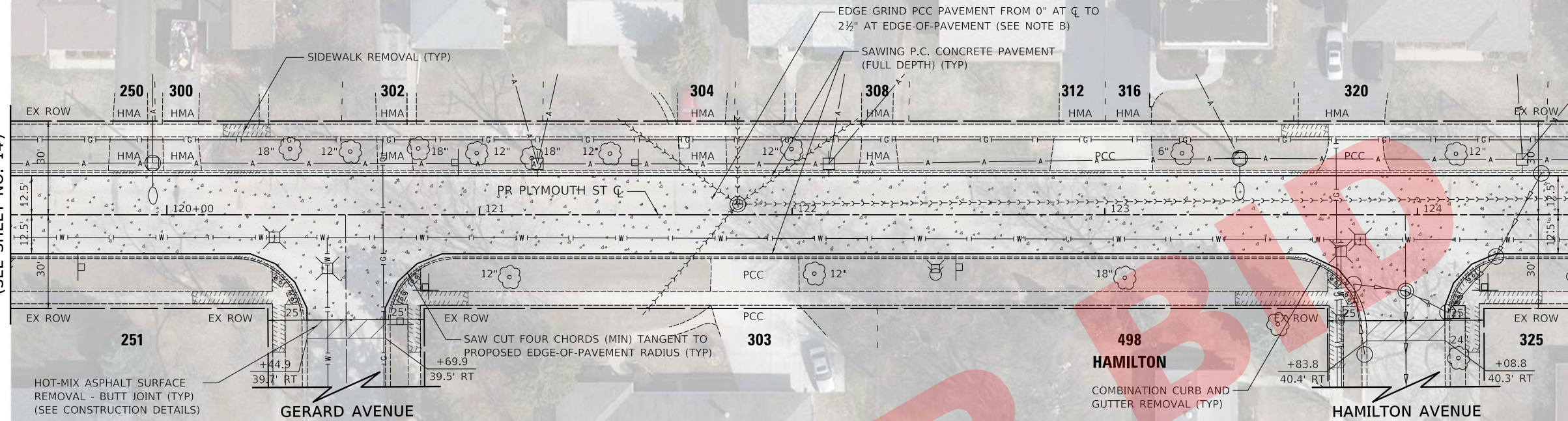
PLYMOUTH STREET  
ROADWAY REMOVAL AND PROPOSED PLAN

SCALE: 1" = 20' SHEET OF SHEETS STA. 114+50 TO STA. 119+50

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DUPAGE	37	14
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				



MATCH LINE STA. 119+50  
(SEE SHEET NO. 14)



MATCH LINE STA. 124+50  
(SEE SHEET NO. 16)

**LEGEND**

## - HOUSE NUMBER

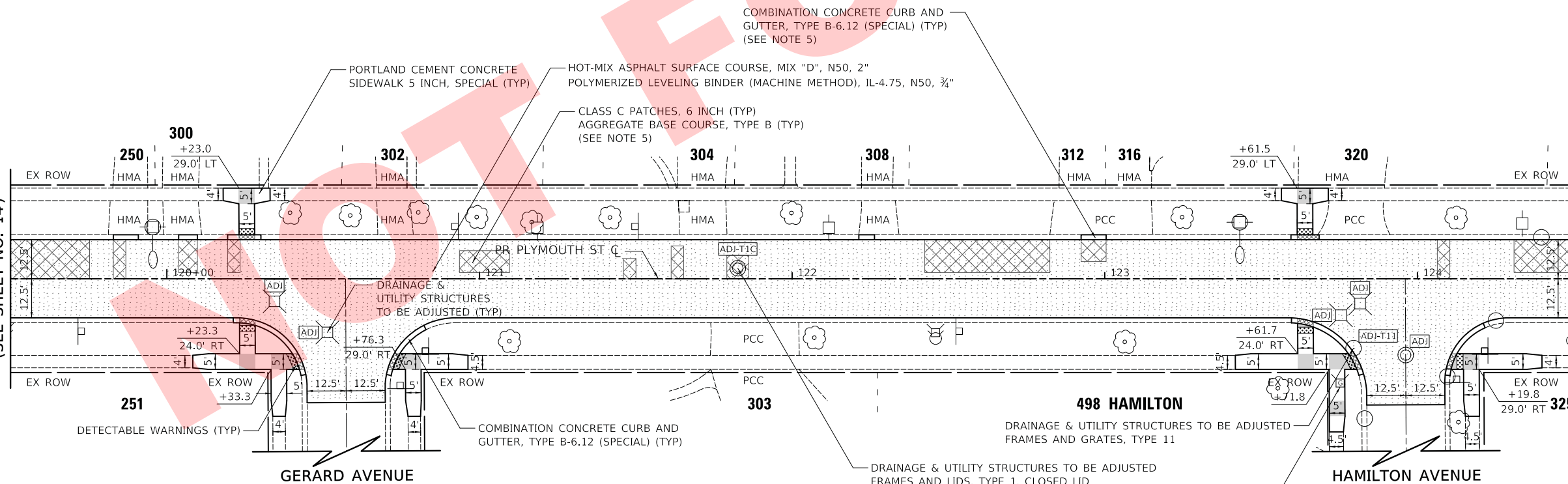
**NOTES**

- B. EDGE GRIND PAID FOR AS: PORTLAND CEMENT CONCRETE SURFACE REMOVAL (VARIABLE DPETH)

PLYMOUTH STREET - REMOVAL PLAN



MATCH LINE STA. 119+50  
(SEE SHEET NO. 14)



MATCH LINE STA. 124+50  
(SEE SHEET NO. 16)

**LEGEND**

## - HOUSE NUMBER

**NOTES**

- 5. THE LOCATIONS AND DIMENSIONS OF ALL PAVEMENT PATCHING AND CURB AND GUTTER REPLACEMENT SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD. FOR ADDITIONAL DETAILS, SEE CONSTRUCTION SEQUENCE NOTES AND OTHER RELEVANT NOTES IN THE MAINTENANCE OF TRAFFIC AND GENERAL NOTES.

PLYMOUTH STREET - PROPOSED PLAN

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	DATE - 06/21/19	REVISED -

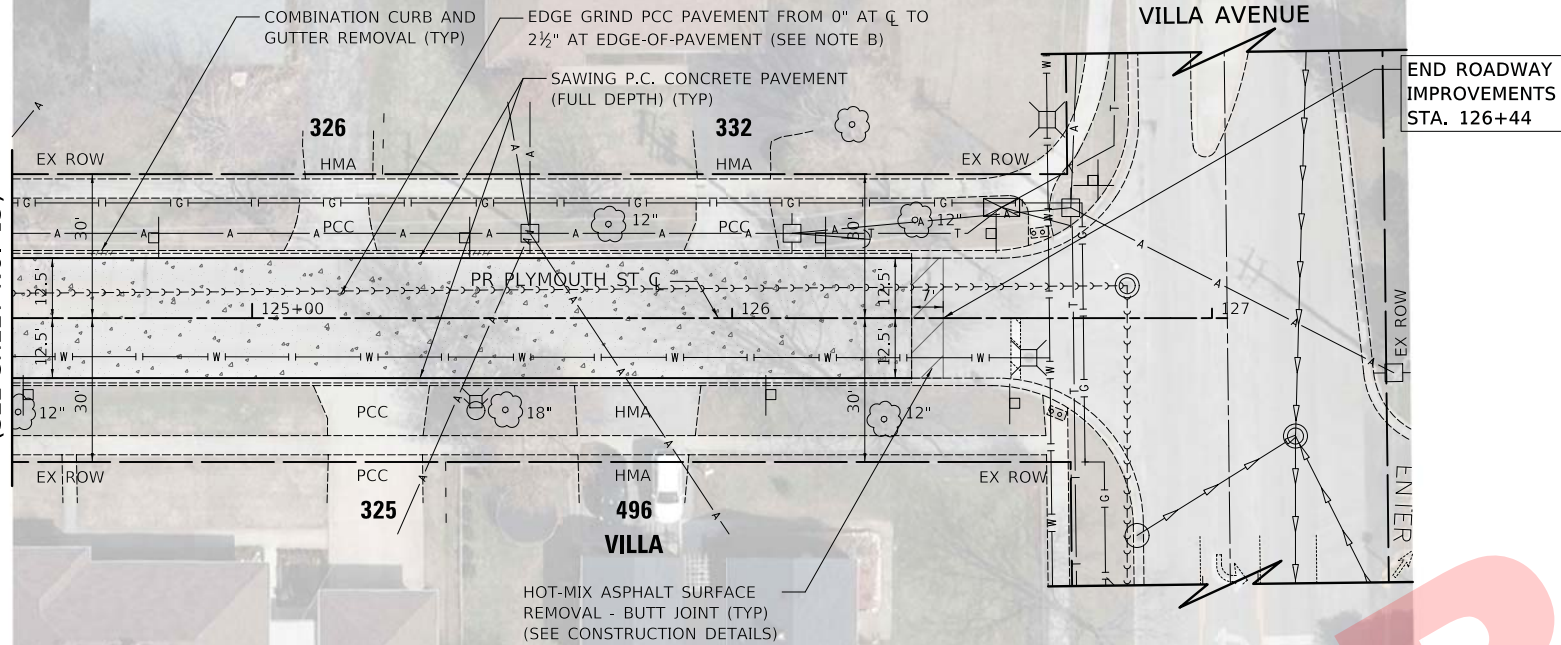
**VILLAGE OF VILLA PARK**

**PLYMOUTH STREET  
ROADWAY REMOVAL AND PROPOSED PLAN**

SCALE: 1" = 20' SHEET OF SHEETS STA. 119+50 TO STA. 124+50

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DUPAGE	37	15
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

MATCH LINE STA. 124+50  
(SEE SHEET NO. 15)



**LEGEND**

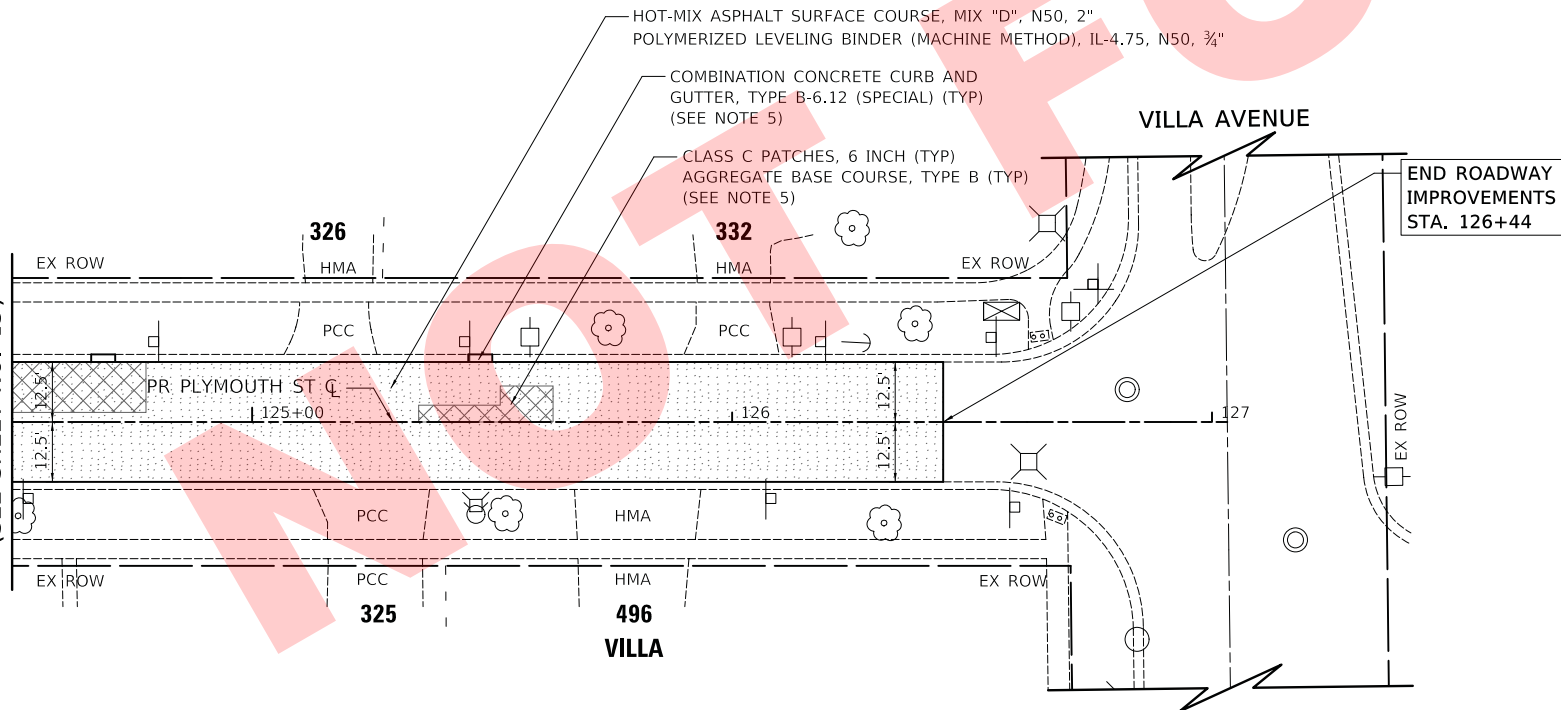
## - HOUSE NUMBER

**NOTES**

- B. EDGE GRIND PAID FOR AS: PORTLAND CEMENT CONCRETE SURFACE REMOVAL (VARIABLE DPETH)

PLYMOUTH STREET - REMOVAL PLAN

MATCH LINE STA. 124+50  
(SEE SHEET NO. 15)



**LEGEND**

## - HOUSE NUMBER

**NOTES**

- 5. THE LOCATIONS AND DIMENSIONS OF ALL PAVEMENT PATCHING AND CURB AND GUTTER REPLACEMENT SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD. FOR ADDITIONAL DETAILS, SEE CONSTRUCTION SEQUENCE NOTES AND OTHER RELEVANT NOTES IN THE MAINTENANCE OF TRAFFIC AND GENERAL NOTES.

PLYMOUTH STREET - PROPOSED PLAN

**PLYMOUTH STREET CROSS-SLOPE TRANSITION TABLE**

**EASTBOUND MOVEMENT**

STATION	PR PGL ELEVATION	CROSS SLOPE	WIDTH FROM PGL TO EOP (RT)	PR EOP ELEVATION (RT OF PGL)
100+50	689.75			MATCH EXISTING
100+55	689.74	-2.50%	12.0	689.44
102+50	690.57	-2.50%	12.5	690.26
103+25	690.89	-3.50%	12.5	690.45
103+61.39	690.94			MATCH EXISTING
103+85	690.75	-2.00%	15.0	690.45
104+15	690.53	-4.00%	12.5	690.03
106+40	689.51	-4.00%	12.5	689.01
106+75	689.63	-3.50%	14.89	689.11
106+99.73	689.70			MATCH EXISTING
110+39.45	689.89			MATCH EXISTING
110+60	689.52	-1.00%	18.12	689.34
110+70	689.34	-2.00%	13.07	689.08
111+65	687.84	-2.00%	12.5	687.59
111+80	687.61	-3.00%	12.5	687.24
113+55	685.53	-3.00%	15.15	685.08
113+78.90	685.44			MATCH EXISTING
126+44				MATCH EXISTING

**PLYMOUTH STREET CROSS-SLOPE TRANSITION TABLE**

**WESTBOUND MOVEMENT**

STATION	PR PGL ELEVATION	CROSS SLOPE	WIDTH FROM PGL TO EOP (LT)	PR EOP ELEVATION (LT OF PGL)
100+50	689.75			MATCH EXISTING
100+55	689.74	-2.00%	12.0	689.50
102+95	690.76	-2.00%	12.5	690.51
103+25	690.89	-4.00%	12.5	690.39
103+61.39	690.94			MATCH EXISTING
103+85	690.75	-1.00%	14.95	690.60
104+00	690.64	-2.00%	12.5	690.39
106+75	689.63	-2.00%	15.62	689.32
106+99.73	689.70			MATCH EXISTING
110+39.45	689.89			MATCH EXISTING
110+60	689.52	-2.00%	17.39	689.17
110+85	689.09	-2.00%	12.5	688.84
111+00	688.86	-3.00%	12.5	688.49
112+20	686.98	-3.00%	12.5	686.61
112+35	686.75	-2.00%	12.5	686.50
113+35	685.69	-2.00%	12.5	685.44
113+50	685.55	-3.00%	13.26	685.15
113+78.90	685.44			MATCH EXISTING
126+44				MATCH EXISTING

**CROSS-SLOPE TRANSITION TABLE**

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FILE NAME = ...



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	DRAWN - MC	REVISED -
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PLOT DATE = 6/21/2019	DATE - 06/21/19	REVISED -

VILLAGE OF VILLA PARK

PLYMOUTH STREET  
ROADWAY REMOVAL AND PROPOSED PLAN

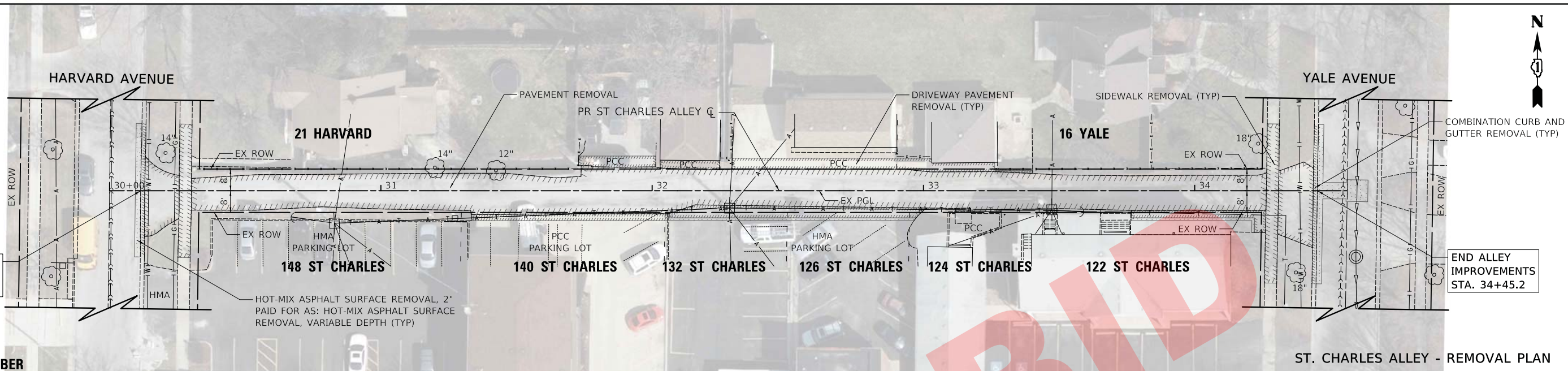
SCALE: 1" = 20' SHEET OF SHEETS STA. 124+50 TO STA. 126+44

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			37	16
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

DATE	
BY	
PLAN	
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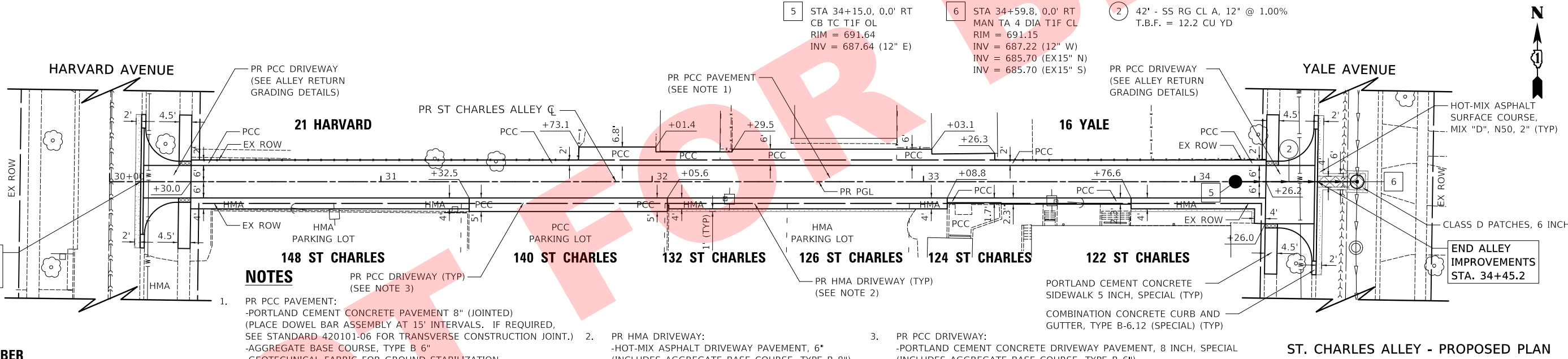
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**LEGEND**

## - HOUSE NUMBER

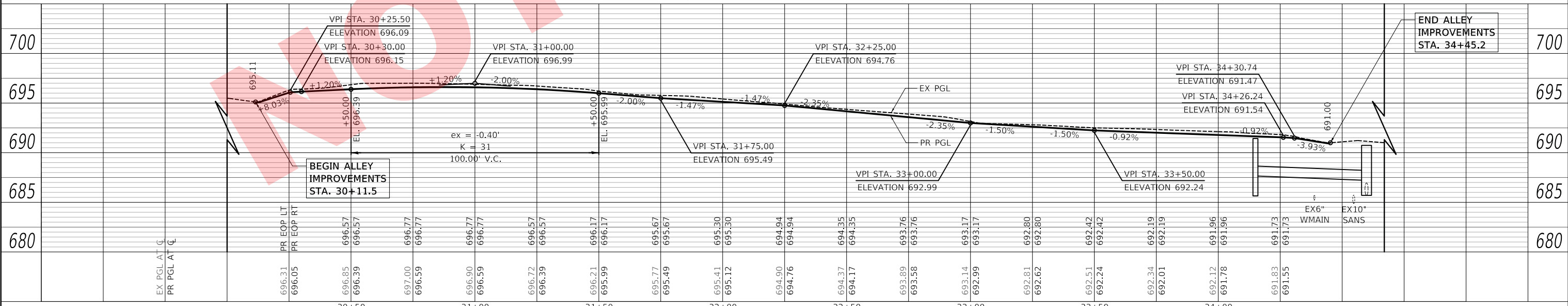


**LEGEND**

## - HOUSE NUMBER

- NOTES**
- PR PCC PAVEMENT:
    - PORTLAND CEMENT CONCRETE PAVEMENT 8" (JOINTED) (PLACE DOWEL BAR ASSEMBLY AT 15' INTERVALS. IF REQUIRED, SEE STANDARD 420101-06 FOR TRANSVERSE CONSTRUCTION JOINT.)
    - AGGREGATE BASE COURSE, TYPE B 6"
    - GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
  - PR HMA DRIVEWAY:
    - HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 6" (INCLUDES AGGREGATE BASE COURSE, TYPE B 8")
  - PR PCC DRIVEWAY:
    - PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH, SPECIAL (INCLUDES AGGREGATE BASE COURSE, TYPE B 6")

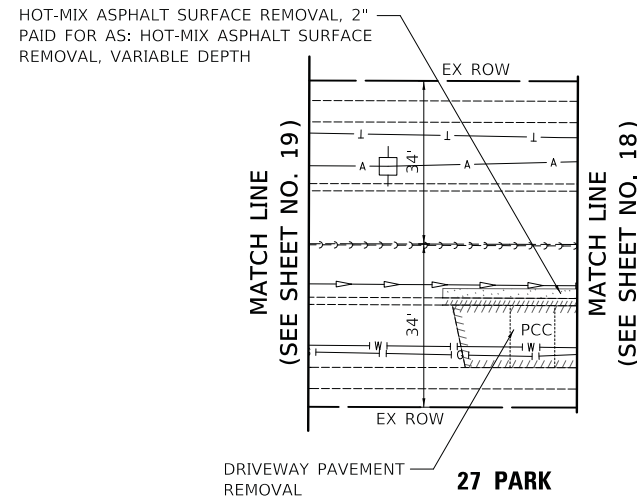
- 5 STA 34+15.0, 0.0' RT  
CB TC T1F OL  
RIM = 691.64  
INV = 687.64 (12" E)
- 6 STA 34+59.8, 0.0' RT  
MAN TA 4 DIA T1F CL  
RIM = 691.15  
INV = 687.22 (12" W)  
INV = 685.70 (EX15" N)  
INV = 685.70 (EX15" S)
- 2 42' - SS RG CL A, 12" @ 1.00%  
T.B.F. = 12.2 CU YD



USER NAME = mjp	DESIGNED - MJP	REVISED -	VILLAGE OF VILLA PARK	ST. CHARLES ALLEY		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
PLOT SCALE = 20.0000' / in.	DRAWN - MC	REVISED -		ROADWAY REMOVAL, PROPOSED PLAN AND PROFILE			DUPAGE	ILLINOIS	37	17
PLOT DATE = 6/21/2019	CHECKED - JRV	REVISED -		SCALE: 1" = 20'		SHEET	OF	SHEETS	STA. 30+11.5	TO STA. 34+45.2



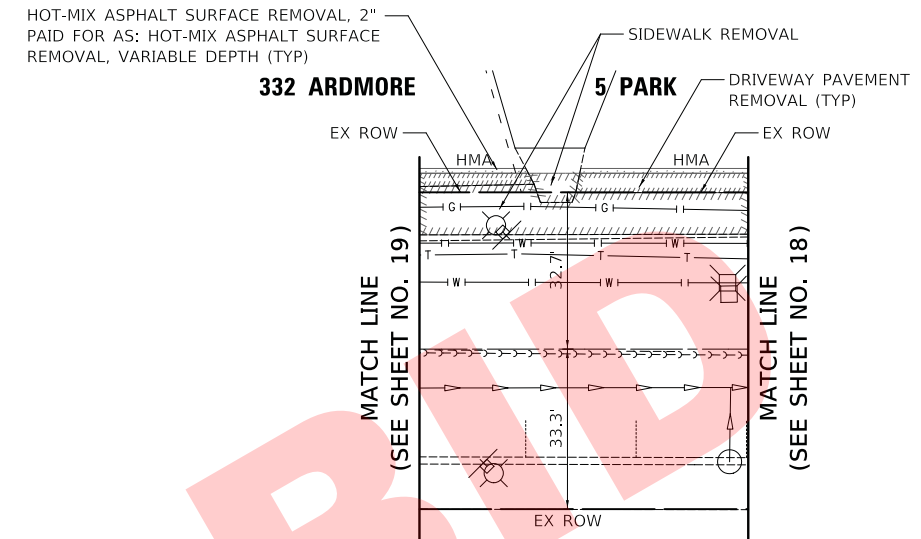
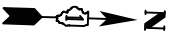




**LEGEND**

## - HOUSE NUMBER

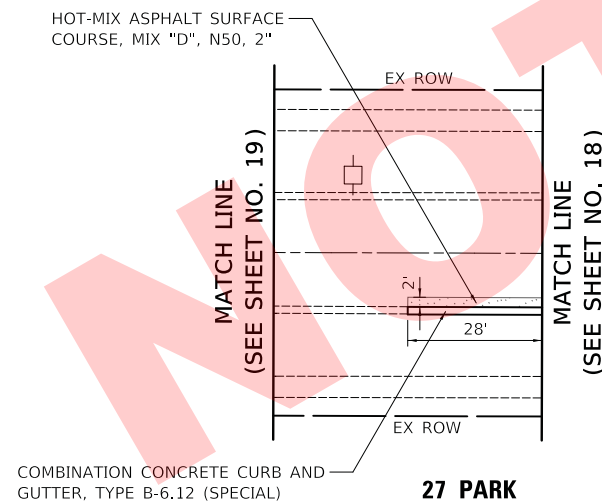
PRINCETON AVENUE - REMOVAL PLAN



**LEGEND**

## - HOUSE NUMBER

ARDMORE AVENUE - REMOVAL PLAN



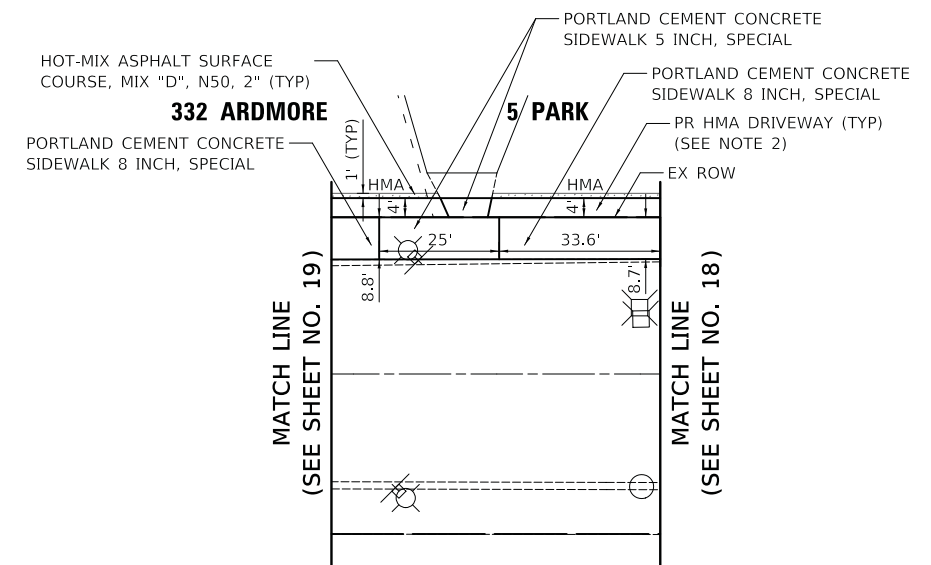
**NOTES**

- 2. PR HMA DRIVEWAY:  
-HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 6"  
(INCLUDES AGGREGATE BASE COURSE, TYPE B 8")

**LEGEND**

## - HOUSE NUMBER

PRINCETON AVENUE - PROPOSED PLAN



**NOTES**

- 2. PR HMA DRIVEWAY:  
-HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 6"  
(INCLUDES AGGREGATE BASE COURSE, TYPE B 8")

**LEGEND**

## - HOUSE NUMBER

ARDMORE AVENUE - PROPOSED PLAN

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PLOT DATE = 6/21/2019	CHECKED - JRV	REVISED -
	DATE - 06/21/19	REVISED -

VILLAGE OF VILLA PARK

PRINCETON AVENUE AND ARDMORE AVENUE  
ROADWAY REMOVAL AND PROPOSED PLAN

SCALE: 1" = 20' SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DUPAGE	37	20
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

**MAINTENANCE OF TRAFFIC GENERAL NOTES**

1. DRUMS EQUIPPED WITH ONE-WAY FLASHING LIGHTS WILL BE REQUIRED AT ALL OPEN TRENCHES, EXCAVATIONS, OPEN OR EXPOSED SEWER STRUCTURES, AND AT ANY OTHER LOCATIONS DESIGNATED BY THE ENGINEER OR LAW ENFORCEMENT AGENCIES. BARRICADES SHALL BE PLACED AT 50' CENTERS ALONG TANGENTS, 20' CENTERS ALONG TAPERS, AND 10' CENTERS IN CURVES AND RADII.
2. "FRESH OIL" SIGNS (W21-2-4848) WITH DATE SIGNS SHALL BE ERECTED 48 HOURS PRIOR TO PRIMING ALONG ALL STREETS. THE COST OF THESE SIGNS SHALL BE INCLUDED IN THE COST OF "TRAFFIC CONTROL AND PROTECTION, (SPECIAL)".
3. THE COST OF SUPPLYING, ERECTING, AND MAINTAINING BARRICADES, DRUMS, WARNING LIGHTS, AND SIGNS SHALL BE INCLUDED IN THE COST OF "TRAFFIC CONTROL AND PROTECTION, (SPECIAL)".
4. ONLY ONE-HALF OF THE ROADWAY WIDTH CAN BE PATCHED AT A TIME.
5. THE CURB AND GUTTER CAN ONLY BE REPLACED ON ONE SIDE OF THE STREET AT A TIME.



**DRIVEWAY ACCESS NOTES**

MAINTAINING ACCESS TO DRIVEWAYS IS OF THE UTMOST IMPORTANCE TO THE VILLAGE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR SUB-STAGING THE CONSTRUCTION OF DRIVEWAYS. THE CONTRACTOR SHALL FOLLOW THESE PROCEDURES TO ENSURE PROPER DRIVEWAY ACCESS:

1. THE CONTRACTOR SHALL WORK WITH ADJACENT BUSINESS AND RESIDENTIAL OWNERS TO DETERMINE DRIVEWAY RECONSTRUCTION SCHEDULING. ALL DRIVEWAY CLOSURES SHALL BE APPROVED BY THE ENGINEER. DRIVEWAY CLOSURES SHALL NOT EXCEED 14 CALENDAR DAYS IN TOTAL FOR ANY INDIVIDUAL PROPERTY.
2. TEMPORARY DRIVES SHALL BE CONSTRUCTED TO THE WIDTH AND AT LOCATIONS DETERMINED BY THE ENGINEER. THE COST OF PLACING, MAINTAINING, AND REMOVING TEMPORARY DRIVES SHALL BE INCLUDED IN THE COST OF "TEMPORARY ACCESS (DRIVEWAY ENTRANCE)".
3. THE TEMPORARY DRIVEWAYS SHALL NOT BE REMOVED FOR THE PLACEMENT OF THE AGGREGATE BASE COURSE OR HOT-MIX ASPHALT BINDER UNTIL THE MORNING PRIOR TO THEIR PLACEMENT. REPLACEMENT OF THE TEMPORARY ACCESS UPON COMPLETION OF THE PLACEMENT OF THE AGGREGATE BASE COURSE OR PAVING OPERATION SHALL BE INCLUDED IN THE COST OF "TEMPORARY ACCESS (DRIVEWAY ENTRANCE)."
4. THE CONTRACTOR SHALL STAGE CONSTRUCT, WHERE WIDTH ALLOWS, CURB AND GUTTER AND DRIVEWAYS AS DIRECTED BY THE ENGINEER. ALL EFFORTS REQUIRED TO SUB-STAGE DRIVEWAY CONSTRUCTION AND MAINTAIN ACCESS TO DRIVEWAY SHALL BE INCLUDED IN THE COST OF "TRAFFIC CONTROL AND PROTECTION, (SPECIAL)".

**SIDEWALK MAINTENANCE NOTES**

1. THE SIDEWALK ON ONE SIDE OF THE STREET MUST REMAIN OPEN AND ACCESSIBLE AT ALL TIMES. UTILITY RELOCATIONS SHALL BE COORDINATED WITH THE ENGINEER AND CONTRACTOR TO ENSURE ONE SIDEWALK REMAINS OPEN. SIGNING DIRECTING PEDESTRIANS TO THE OPEN SIDEWALK SHALL BE IN ACCORDANCE WITH IDOT HIGHWAY STANDARD 701801-06. THE WORK REQUIRED TO COMPLY WITH THESE REQUIREMENTS SHALL BE INCLUDED IN THE COST OF "TRAFFIC CONTROL AND PROTECTION, (SPECIAL)."
2. TEMPORARY AGGREGATE REQUIRED TO MAINTAIN PEDESTRIAN ACCESS ACROSS THE WORKZONE SHALL BE INCLUDED IN THE COST OF "TEMPORARY ACCESS (ROAD)".

**ALLEY DUMPSTER ACCESS NOTES**

1. ACCESS TO DUMPSTERS IN THE ALLEYS SHALL BE MAINTAINED AT ALL TIMES THROUGHOUT THE DURATION OF CONSTRUCTION. IF TRASH HAULERS ARE UNABLE TO ACCESS DUMPSTERS, THE CONTRACTOR SHALL RELOCATE DUMPSTERS TO A LOCATION ACCESSIBLE BY TRASH HAULERS. THE CONTRACTOR SHALL ALSO RETURN DUMPSTERS TO THEIR ORIGINAL LOCATION FOLLOWING PICKUP BY THE TRASH HAULER.
2. THE CONTRACTOR SHALL COORDINATE WITH ENGINEER AND PROPERTY OWNER ON ACCEPTABLE LOCATIONS WHERE DUMPSTERS MAY BE RELOCATED WHEN ACCESSIBILITY IS NOT POSSIBLE FROM THE ALLEY. THIS WORK AND COORDINATION WILL BE INCLUDED IN THE COST OF "TRAFFIC CONTROL AND PROTECTION, (SPECIAL)".

**CONSTRUCTION SEQUENCE**

THIS CONSTRUCTION SEQUENCE WAS DEVELOPED TO MINIMIZE DISRUPTION OF ACCESS TO HOMEOWNERS AND BUSINESSES DRIVEWAYS. THIS CONSTRUCTION SEQUENCE SHALL BE FOLLOWED UNLESS ALTERNATE SEQUENCE IS APPROVED BY THE VILLAGE.

**ALL ROADWAYS & ALLEYS**

- CALL JULIE
- PERFORM PRECONSTRUCTION VIDEO TAPING
- SET UP TRAFFIC CONTROL PER APPLICABLE IDOT STANDARDS
- SET UP EROSION AND SEDIMENT CONTROL MEASURES

**PLYMOUTH STREET (RECONSTRUCTION AREAS)**

1. REMOVE PAVEMENT, DRIVEWAY APRONS AND CURB AND GUTTER
2. COMPLETE DRAINAGE AND UTILITY WORK
3. INSTALL GEOTECHNICAL FABRIC, AGGREGATE SUBGRADE IMPROVEMENT (WHERE REQUIRED) AND AGGREGATE BASE COURSE
4. INSTALL CURB AND GUTTER
5. INSTALL HMA BINDER
6. INSTALL DRIVEWAY APRONS
7. REMOVE AND REPLACE SIDEWALK AT INTERSECTIONS TO CONFORM TO ADA GRADING REQUIREMENTS
8. LANDSCAPE
9. INSTALL HMA SURFACE COURSE
10. INSTALL PAVEMENT MARKINGS

**PLYMOUTH STREET (RESURFACING AREAS)**

1. COMPLETE DRAINAGE AND UTILITY WORK
2. SAW CUT MONOLITHIC PAVEMENT TO SEPARATE CURB AND GUTTER
3. REMOVE AND REPLACE CURB AND GUTTER AS DIRECTED BY THE ENGINEER
4. REMOVE UTILITY FRAMES FROM PAVEMENT AREAS
5. EDGE GRIND CONCRETE SURFACE ACROSS ENTIRE LANE
6. PATCH PAVEMENT BASE AS DIRECTED BY THE ENGINEER
7. INSTALL HMA LEVELING BINDER
8. REMOVE AND REPLACE SIDEWALK AT INTERSECTIONS TO CONFORM TO ADA GRADING REQUIREMENTS
9. LANDSCAPE
10. RE-INSTALL UTILITY FRAMES
11. INSTALL HMA SURFACE COURSE
12. INSTALL PAVEMENT MARKINGS

**PARK BOULEVARD**

1. COMPLETE DRAINAGE AND UTILITY WORK
2. REMOVE 1' OF SIDEWALK BEHIND CURB AND GUTTER ON SOUTH SIDE AND REMOVE SIDEWALK ON NORTH SIDE AS DIRECTED BY THE ENGINEER
3. REMOVE AND REPLACE CURB AND GUTTER ON BOTH SIDES
4. INSTALL SIDEWALK
5. REMOVE UTILITY FRAMES FROM PAVEMENT AREAS
6. REMOVE HMA SURFACE COURSE
7. PATCH PAVEMENT BASE AS DIRECTED BY THE ENGINEER
8. INSTALL HMA LEVELING BINDER
9. LANDSCAPE
10. RE-INSTALL UTILITY FRAMES
11. INSTALL HMA SURFACE COURSE
12. INSTALL PAVEMENT MARKINGS

**ALLEYS**

1. REMOVE ALLEY PAVEMENT AND DRIVEWAY PAVEMENT
2. INSTALL GEOTECHNICAL FABRIC, AGGREGATE SUBGRADE IMPROVEMENT (WHERE REQUIRED) AND AGGREGATE BASE COURSE
3. PLACE DOWEL BAR ASSEMBLY AT 15' INTERVALS
4. INSTALL JOINTED PLAIN CONCRETE PAVEMENT
5. CLOSE ALLEY FOR CONCRETE TO CURE
6. INSTALL DRIVEWAY APRONS AND OTHER APPURTENANCES
7. LANDSCAPE
8. INSTALL PAVEMENT MARKINGS (WHERE NECESSARY)

PLOT DRIVER = ...\\dm04986\ora\pdf\cvt  
PEN TABLE = ...\\dm04986\ora\pdf\cvt  
FILE NAME = ...\\dm04986\ora\pdf\cvt



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	DRAWN - MJP	REVISED -
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PLOT DATE = 6/21/2019	DATE - 06/21/19	REVISED -

VILLAGE OF VILLA PARK

2019 STREET IMPROVEMENT PROJECT  
MAINTENANCE OF TRAFFIC - GENERAL NOTES

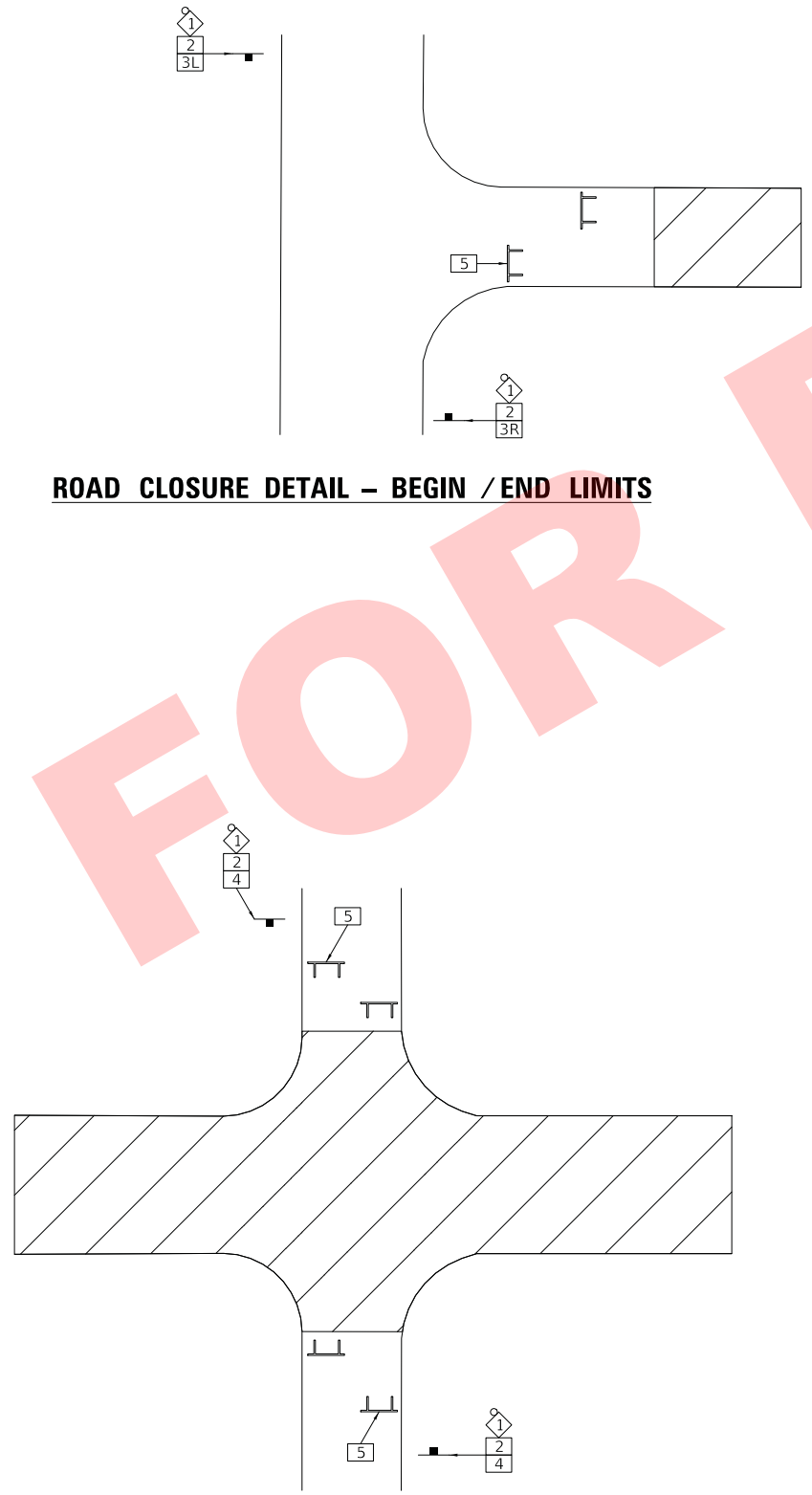
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			37	21
CONTRACT NO.				
ILLINOIS		FED. AID PROJECT		

**ROAD CLOSURE GENERAL NOTES**

1. THE ENGINEER SHALL BE NOTIFIED IN WRITING AT LEAST 7 CALENDAR DAYS PRIOR TO THE DAY THE ROAD CLOSURE IS TO BE IN EFFECT. THE CONTRACTOR SHALL CONTACT THE APPROPRIATE LOCAL AGENCIES AND INTERESTED PARTIES.
2. ALL SIGNING SHALL BE IN ACCORDANCE WITH THE APPLICABLE PROVISIONS OF THE STANDARD SPECIFICATIONS, THE DETAILS IN THESE PLANS, THE LATEST EDITION OF THE STATE OF ILLINOIS "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES," AND AS DIRECTED BY THE ENGINEER.
3. THE SIZES OF ALL SIGNS NOT SPECIFIED IN THESE PLANS SHALL BE AS REQUIRED BY THE FHWA "MANUAL ON UNIFORM TRAFFIC CONTRL DEVICES" AND ILLINOIS SUPPLEMENT. ADDITIONAL SIGNING AND/OR BARRICADES DEEMED NECESSARY BY THE ENGINEER SHALL BE PROVIDED AND INSTALLED. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF "TRAFFIC CONTROL AND PROTECTION, (SPECIAL)".
4. THE CONTRACTOR SHALL PROVIDE THE ENGINEER WITH THE NAMES AND PHONE NUMBERS OF HIS REPRESENTATIVES ON THE CONSTRUCTION SITE, AND HIS REPRESENTATIVE RESPONSIBLE FOR THE ROAD SIGNING, SEVEN CALENDAR DAYS PRIOR TO THE START OF WORK.
5. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE FIELD LOCATION OF ALL ROAD CLOSURE AND CONSTRUCTION SIGNING. THE CONTRACTOR MAY REQUEST THE ENGINEER TO FIELD VERIFY THE POSITIONS OF ANY SIGNS.
6. ACTUAL LOCATIONS FOR SIGNING SHOWN ON THE ROAD CLOSURE PLANS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. ALL EXISTING SIGNING THAT IS NOT APPLICABLE WHILE THE ROAD CLOSURE IS IN EFFECT SHALL BE COMPLETELY COVERED BY THE CONTRACTOR IN A MANNER MEETING THE APPROVAL OF THE ENGINEER.
8. ALL ROAD CLOSURE SIGNING SHALL BE POST MOUNTED.
- 9.. ALL ROAD CLOSURE SIGNING EXCEPT REGULATORY SIGNS SHALL HAVE BLACK LEGENDS ON FLUORESCENT ORANGE SHEETING AND STANDARD BLACK BORDERS. THE FLUORESCENT ORANGE REFLECTIVE SHEETING SHALL MEET THE REQUIREMENTS OF ARTICLE 1106.01 OF THE STANDARD SPECIFICATIONS. ALL ROAD CLOSURE SIGNING SHALL BE NEW OR IN LIKE-NEW CONDITION. THE ENGINEER SHALL BE THE SOLE JUDGE OF THE CONDITION OF THE SIGNS.
10. THE ROAD NAME SIGN SHALL BE A BLACK LEGEND ON ORANGE REFLECTIVE SHEETING. THE SIGN BLANK SHALL BE VARIABLE WITH DESIGN SERIES B LETTERS. THE CAPITAL LETTERS SHALL BE 4 INCHES.
11. AT A MINIMUM, ALL AMBER FLASHING LIGHTS THAT ARE REQUIRED FOR THE ROAD CLOSURE SIGNING SHALL MEET THE REQUIREMENTS FOR TYPE A-LOW INTENSITY FLASHING LIGHTS IN ARTICLE 1106.02 OF THE STANDARD SPECIFICATIONS. ALL LIGHTS SHALL OPERATE DURING HOURS OF DARKNESS. ONLY LIGHTS THAT HAVE BEEN APPROVED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION SHALL BE USED.
12. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ENSURING THAT ALL BARRICADES, SIGNS, LIGHTS AND OTHER DEVICES INSTALLED BY HIM ARE IN PLACE AND OPERATING 24 HOURS EACH DAY, INCLUDING SUNDAYS AND HOLIDAYS.
13. TYPE III BARRICADES SHALL BE USED AT POINTS OF CLOSURE TO THRU TRAFFIC ONLY AND SHALL NOT EXCEED 8 FEET IN WIDTH EACH FOR A SINGLE APPROACH LANE. ALL BARRICADES AT THESE LOCATIONS SHALL HAVE REFLECTORIZED STRIPING ON THE BACK SIDES OF THE BARRICADES.
14. CONSTRUCTION EQUIPMENT SHALL NOT BE PARKED IMMEDIATELY BEHIND THE TYPE III BARRICADES DURING NON-WORKING HOURS. IN ANY EVENT, ARTICLE 701.11 OF THE STANDARD SPECIFICATIONS SHALL APPLY.
15. DURING NON-WORKING HOURS THE CONTRACTOR SHALL PROVIDE A MEANS TO RESTRAIN THE TYPE III BARRICADES FROM EASY MOVEMENT BY VANDALS. THE CHOSEN METHOD SHALL BE APPROVED BY THE ENGINEER.
16. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING THE VISIBILITY OF ALL ROAD CLOSURE AND CONSTRUCTION SIGNS, INCLUDING BRUSHING BACK VEGETATION IF DEEMED NECESSARY BY THE ENGINEER.
17. THE ENGINEER SHALL BE NOTIFIED AT LEAST 24 HOURS BEFORE THE ROAD IS TO BE REOPENED TO TRAFFIC. THE CONTRACTOR WILL CONTACT THE APPROPRIATE LOCAL AGENCIES AND INTERESTED PARTIES.
18. THE COST OF THE ROAD CLOSURE AS DEFINED ON THE MAINTENANCE OF TRAFFIC - ROAD CLOSURE PLAN SHALL BE INCLUDED IN THE LUMP SUM PRICE FOR "TRAFFIC CONTROL AND PROTECTION, (SPECIAL)".

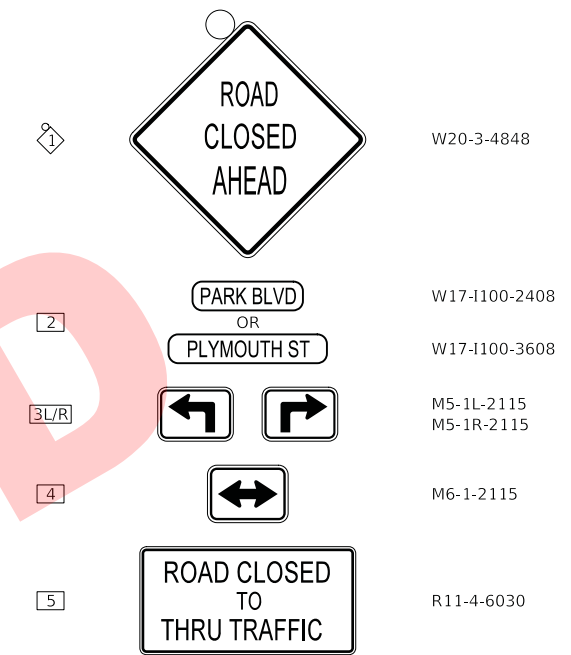
**ROAD CLOSURE DETAIL – BEGIN / END LIMITS**



**ROAD CLOSURE DETAIL – 4-WAY INTERSECTION**

**SIGN NO.**

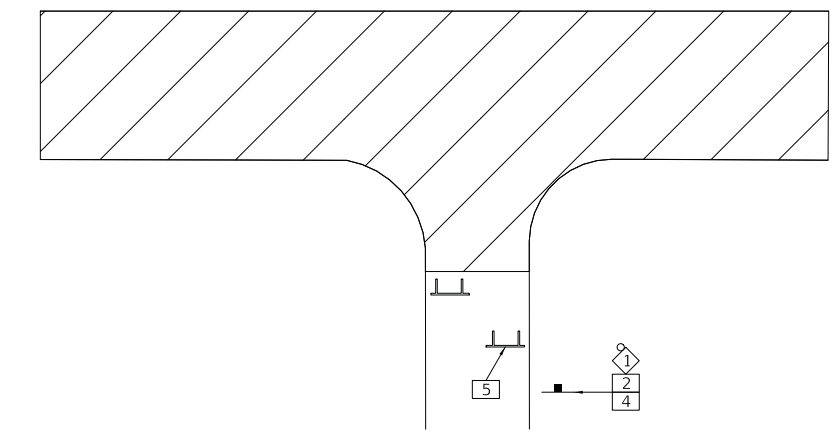
**SIGN TYPE**



**LEGEND**

- [Hatched Box] CONSTRUCTION WORK ZONE
- [Square with T-bar] PROPOSED TRAFFIC SIGN
- [T-bar] TYPE III BARRICADE WITH FLASHING LIGHTS
- [Diamond] CONSTRUCTION WARNING SIGN WITH AMBER FLASHING LIGHT

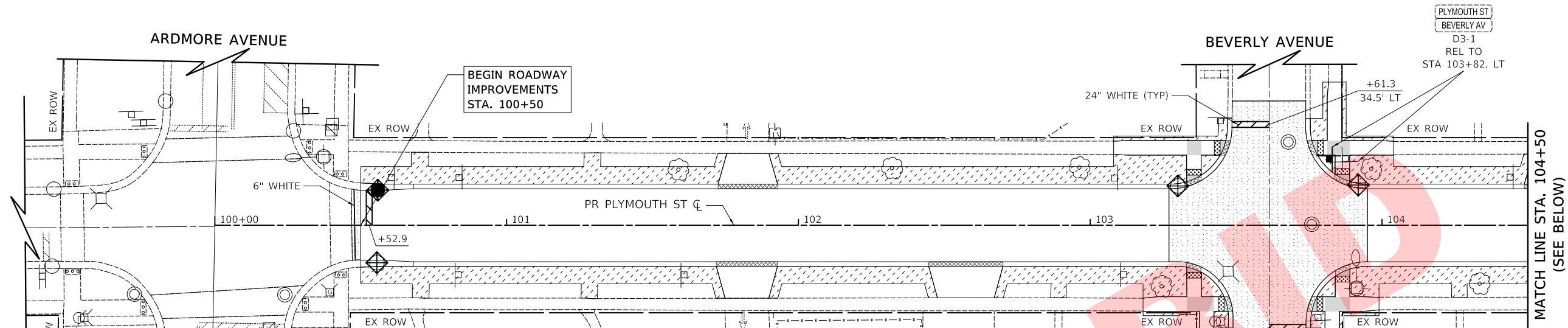
**ROAD CLOSURE DETAIL – 3-WAY INTERSECTION**



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 FILE NAME = ...\\dm04986\cra\pdf\c1t



USER NAME = mjp	DESIGNED - MJP	REVISED -	<b>VILLAGE OF VILLA PARK</b>	<b>2019 STREET IMPROVEMENT PROJECT MAINTENANCE OF TRAFFIC - ROAD CLOSURE DETAILS</b>				F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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PLOT DATE = 6/21/2019	CHECKED - JRV	REVISED -		CONTRACT NO.									
	DATE - 06/21/19	REVISED -		ILLINOIS FED. AID PROJECT									



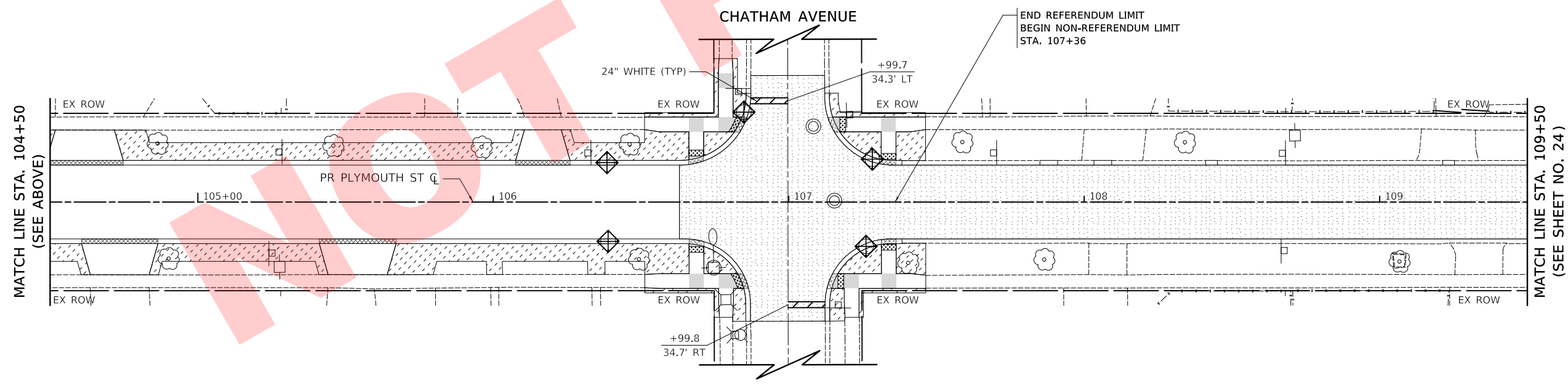
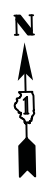
**LEGEND**

- REL REMOVE SIGN PANEL ASSEMBLY - TYPE A  
RELOCATE SIGN PANEL ASSEMBLY - TYPE A  
TELESCOPING STEEL SIGN SUPPORT
- NEW SIGN PANEL, TYPE 1  
TELESCOPING STEEL SIGN SUPPORT
- EX SIGN
- EX SIGN ATTACHED TO POLE
- PR SIGN
- TOPSOIL FURNISH AND PLACE, 4" (SPECIAL)  
SODDING, SPECIAL
- INLET FILTERS

**NOTES**

1. ALL SIGNS ARE EXISTING TO REMAIN UNLESS OTHERWISE NOTED.
2. ALL PROPOSED SIGN FACES SHALL BE TYPE AZ (DIAMOND GRADE) REFLECTORIZED SHEETING. THIS WORK SHALL BE PAID FOR AS "SIGN PANEL" OF THE TYPE SPECIFIED.
3. ALL PERMANENT PAVEMENT MARKINGS SHALL BE THERMOPLASTIC UNLESS OTHERWISE NOTED.
4. EXISTING PAVEMENT MARKINGS IN CONFLICT WITH PROPOSED MARKINGS SHALL BE REMOVED. THE REMOVAL SHALL BE INCLUDED IN THE COST OF THE PROPOSED MARKING BEING INSTALLED.
5. DIMENSIONS TO PAVEMENT MARKINGS ARE TO THE CENTER OF A SINGLE LINE OR THE CENTER OF GAP FOR A DOUBLE LINE.

PLYMOUTH STREET - PAVEMENT MARKING AND LANDSCAPING PLAN



PLYMOUTH STREET - PAVEMENT MARKING AND LANDSCAPING PLAN

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FILE NAME = ...\\dm04986\cra\pff\c1t

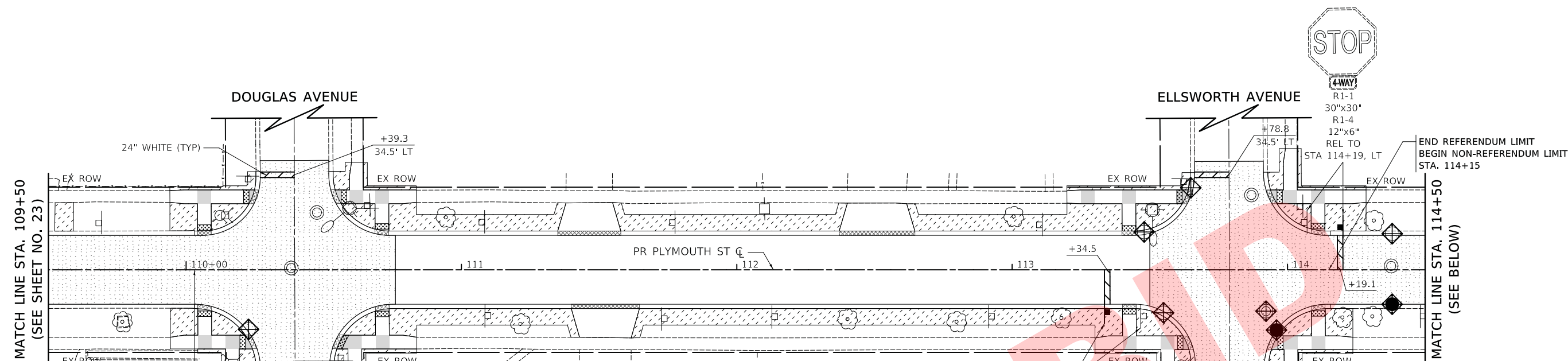


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PLOT DATE = 6/21/2019	CHECKED - JRV	REVISED -
	DATE - 06/21/19	REVISED -

VILLAGE OF VILLA PARK

PLYMOUTH STREET PAVEMENT MARKING AND LANDSCAPING PLAN			
SCALE: 1" = 20'	SHEET	OF SHEETS	STA. 100+50 TO STA. 109+50

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DUPAGE	37	23
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				



MATCH LINE STA. 109+50  
(SEE SHEET NO. 23)

END NON-REFERENDUM LIMIT  
BEGIN REFERENDUM LIMIT  
STA. 110+03

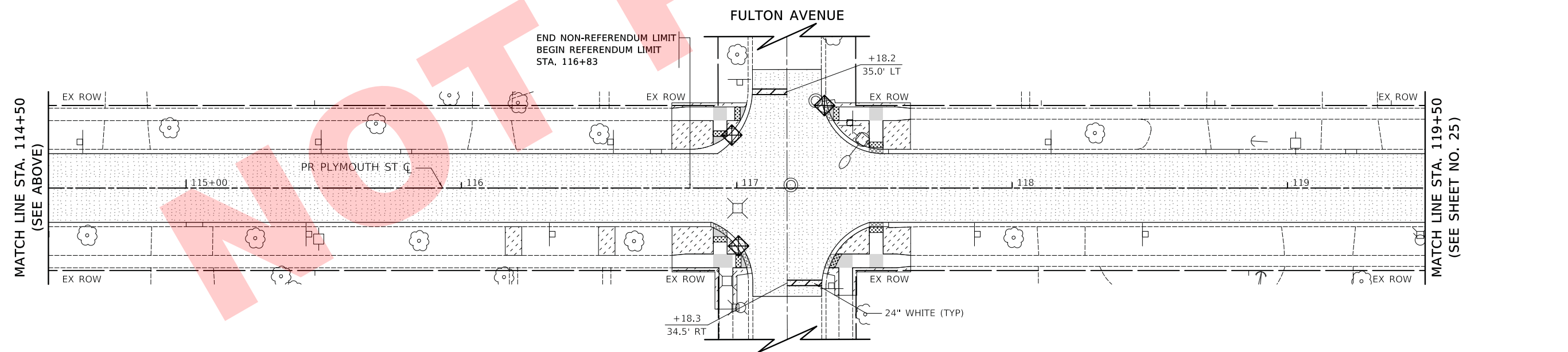
END REFERENDUM LIMIT  
BEGIN NON-REFERENDUM LIMIT  
STA. 114+15

MATCH LINE STA. 114+50  
(SEE BELOW)

- LEGEND**
- REL REMOVE SIGN PANEL ASSEMBLY - TYPE A  
RELOCATE SIGN PANEL ASSEMBLY - TYPE A  
TELESCOPING STEEL SIGN SUPPORT
  - NEW SIGN PANEL, TYPE 1  
TELESCOPING STEEL SIGN SUPPORT
  - EX SIGN
  - EX SIGN ATTACHED TO POLE
  - PR SIGN
  - TOPSOIL FURNISH AND PLACE, 4" (SPECIAL)  
SODDING, SPECIAL
  - INLET FILTERS

- NOTES**
1. ALL SIGNS ARE EXISTING TO REMAIN UNLESS OTHERWISE NOTED.
  2. ALL PROPOSED SIGN FACES SHALL BE TYPE AZ (DIAMOND GRADE) REFLECTORIZED SHEETING. THIS WORK SHALL BE PAID FOR AS "SIGN PANEL" OF THE TYPE SPECIFIED.
  3. ALL PERMANENT PAVEMENT MARKINGS SHALL BE THERMOPLASTIC UNLESS OTHERWISE NOTED.
  4. EXISTING PAVEMENT MARKINGS IN CONFLICT WITH PROPOSED MARKINGS SHALL BE REMOVED. THE REMOVAL SHALL BE INCLUDED IN THE COST OF THE PROPOSED MARKING BEING INSTALLED.
  5. DIMENSIONS TO PAVEMENT MARKINGS ARE TO THE CENTER OF A SINGLE LINE OR THE CENTER OF GAP FOR A DOUBLE LINE.

PLYMOUTH STREET - PAVEMENT MARKING AND LANDSCAPING PLAN



MATCH LINE STA. 114+50  
(SEE ABOVE)

MATCH LINE STA. 119+50  
(SEE SHEET NO. 25)

END NON-REFERENDUM LIMIT  
BEGIN REFERENDUM LIMIT  
STA. 116+83

PLYMOUTH STREET - PAVEMENT MARKING AND LANDSCAPING PLAN

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PEN TABLE = ...  
FILE NAME = ...

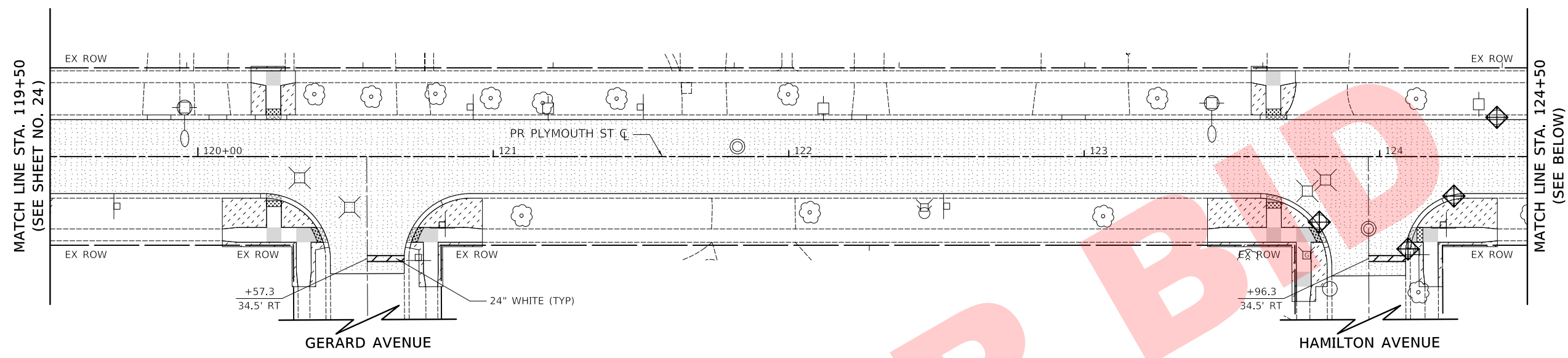


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	DATE - 06/21/19	REVISED -

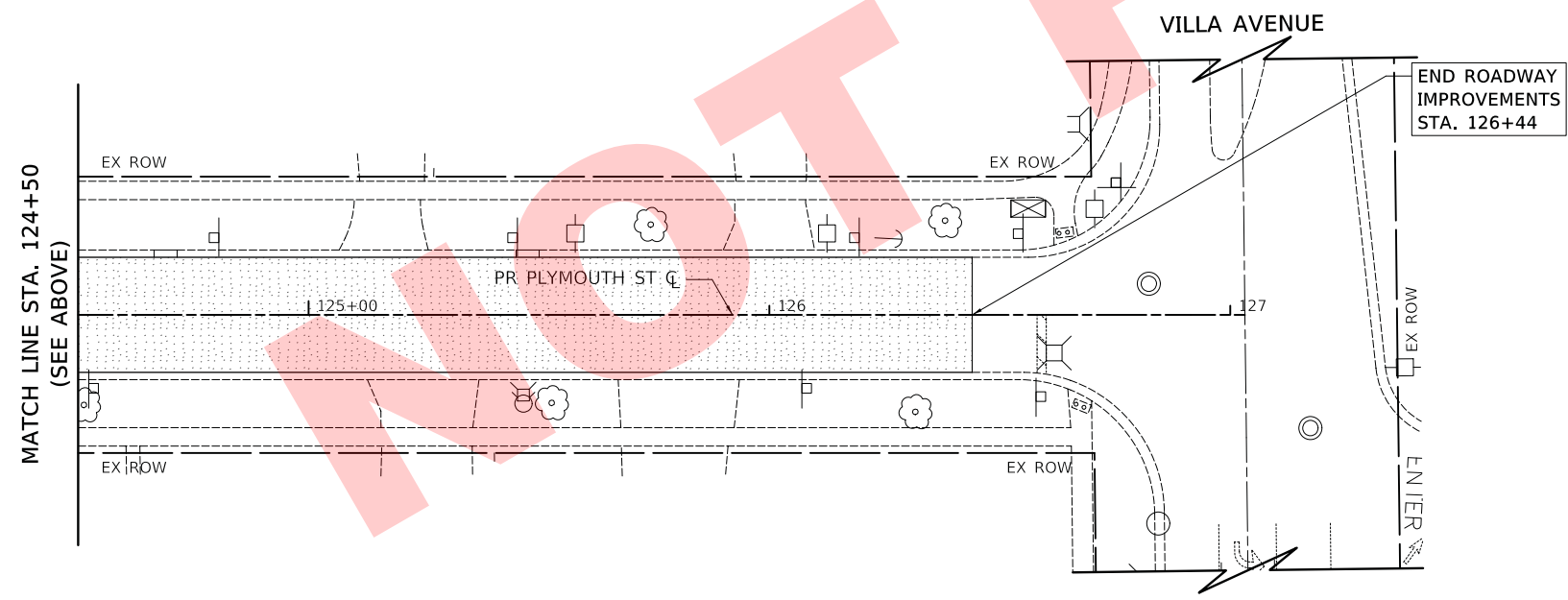
VILLAGE OF VILLA PARK

PLYMOUTH STREET PAVEMENT MARKING AND LANDSCAPING PLAN			
SCALE: 1" = 20'	SHEET	OF SHEETS	STA. 109+50 TO STA. 119+50

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DUPAGE	37	24
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				



PLYMOUTH STREET - PAVEMENT MARKING AND LANDSCAPING PLAN



**LEGEND**

REL REMOVE SIGN PANEL ASSEMBLY - TYPE A  
RELOCATE SIGN PANEL ASSEMBLY - TYPE A  
TELESCOPING STEEL SIGN SUPPORT

NEW SIGN PANEL, TYPE 1  
TELESCOPING STEEL SIGN SUPPORT

EX SIGN

EX SIGN ATTACHED TO POLE

PR SIGN

TOPSOIL FURNISH AND PLACE, 4" (SPECIAL)  
SODDING, SPECIAL

INLET FILTERS

- NOTES**
1. ALL SIGNS ARE EXISTING TO REMAIN UNLESS OTHERWISE NOTED.
  2. ALL PROPOSED SIGN FACES SHALL BE TYPE AZ (DIAMOND GRADE) REFLECTORIZED SHEETING. THIS WORK SHALL BE PAID FOR AS "SIGN PANEL" OF THE TYPE SPECIFIED.
  3. ALL PERMANENT PAVEMENT MARKINGS SHALL BE THERMOPLASTIC UNLESS OTHERWISE NOTED.
  4. EXISTING PAVEMENT MARKINGS IN CONFLICT WITH PROPOSED MARKINGS SHALL BE REMOVED. THE REMOVAL SHALL BE INCLUDED IN THE COST OF THE PROPOSED MARKING BEING INSTALLED.
  5. DIMENSIONS TO PAVEMENT MARKINGS ARE TO THE CENTER OF A SINGLE LINE OR THE CENTER OF GAP FOR A DOUBLE LINE.



PLYMOUTH STREET - PAVEMENT MARKING AND LANDSCAPING PLAN

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PEN TABLE = ...\\vms04986\cra\pff\cjt  
FILE NAME = ...\\vms04986\cra\pff\cjt

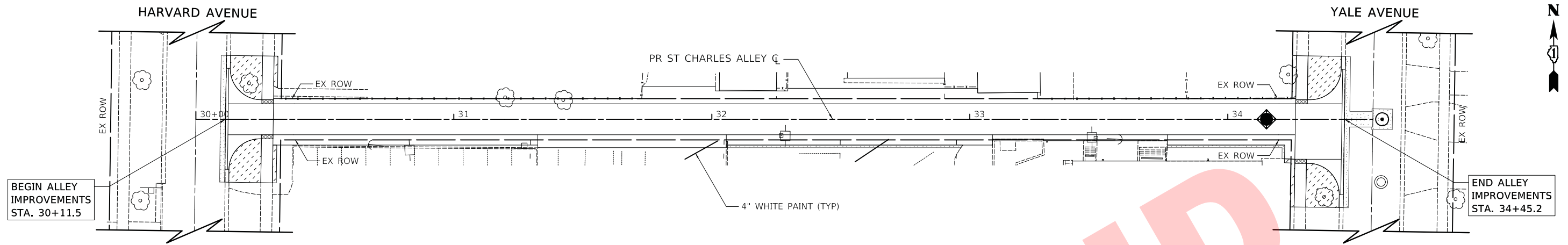


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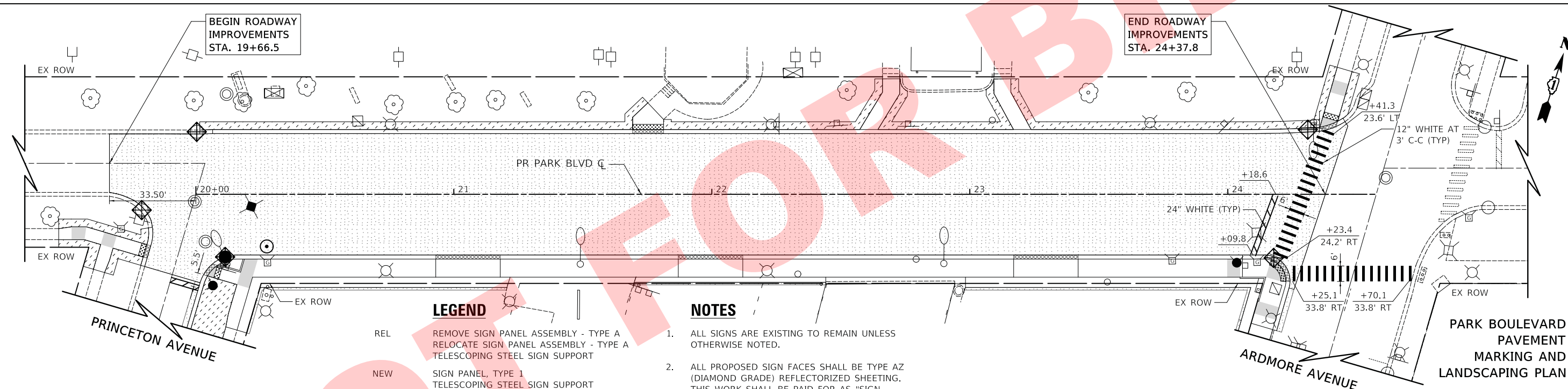
VILLAGE OF VILLA PARK

PLYMOUTH STREET PAVEMENT MARKING AND LANDSCAPING PLAN			
SCALE: 1" = 20'	SHEET	OF SHEETS	STA. 119+50 TO STA. 126+44

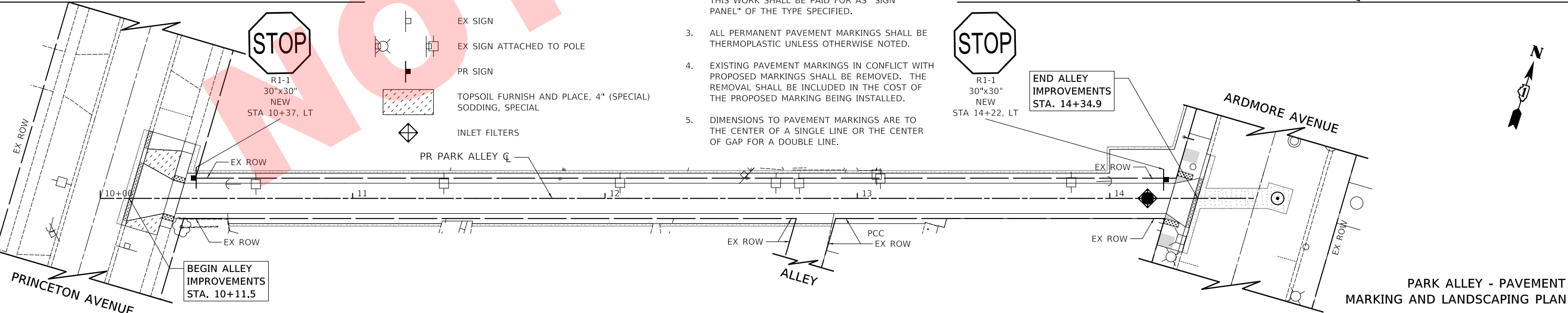
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DUPAGE	37	25
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				



ST. CHARLES ALLEY - PAVEMENT MARKING AND LANDSCAPING PLAN



PARK BOULEVARD PAVEMENT MARKING AND LANDSCAPING PLAN



PARK ALLEY - PAVEMENT MARKING AND LANDSCAPING PLAN

**LEGEND**

REL	REMOVE SIGN PANEL ASSEMBLY - TYPE A RELOCATE SIGN PANEL ASSEMBLY - TYPE A TELESCOPING STEEL SIGN SUPPORT
NEW	SIGN PANEL, TYPE 1 TELESCOPING STEEL SIGN SUPPORT
	EX SIGN
	EX SIGN ATTACHED TO POLE
	PR SIGN
	TOPSOIL FURNISH AND PLACE, 4" (SPECIAL) SODDING, SPECIAL
	INLET FILTERS

- NOTES**
1. ALL SIGNS ARE EXISTING TO REMAIN UNLESS OTHERWISE NOTED.
  2. ALL PROPOSED SIGN FACES SHALL BE TYPE AZ (DIAMOND GRADE) REFLECTORIZED SHEETING. THIS WORK SHALL BE PAID FOR AS "SIGN PANEL" OF THE TYPE SPECIFIED.
  3. ALL PERMANENT PAVEMENT MARKINGS SHALL BE THERMOPLASTIC UNLESS OTHERWISE NOTED.
  4. EXISTING PAVEMENT MARKINGS IN CONFLICT WITH PROPOSED MARKINGS SHALL BE REMOVED. THE REMOVAL SHALL BE INCLUDED IN THE COST OF THE PROPOSED MARKING BEING INSTALLED.
  5. DIMENSIONS TO PAVEMENT MARKINGS ARE TO THE CENTER OF A SINGLE LINE OR THE CENTER OF GAP FOR A DOUBLE LINE.

VILLAGE OF VILLA PARK

ST. CHARLES ALLEY, PARK BOULEVARD AND PARK ALLEY  
PAVEMENT MARKING AND LANDSCAPING PLAN

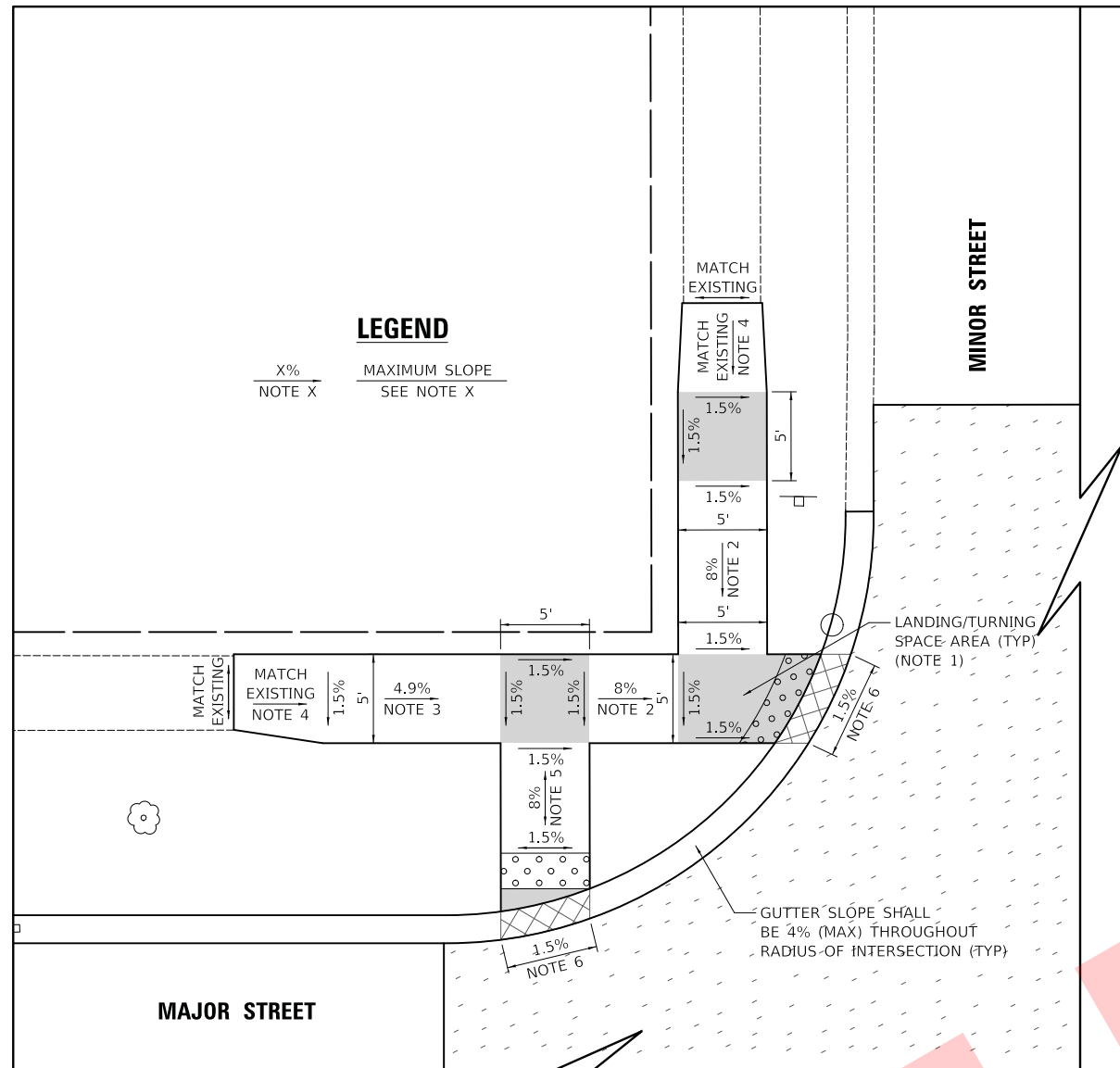
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		DUPAGE	37	26
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

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	DATE - 06/21/19	REVISED -

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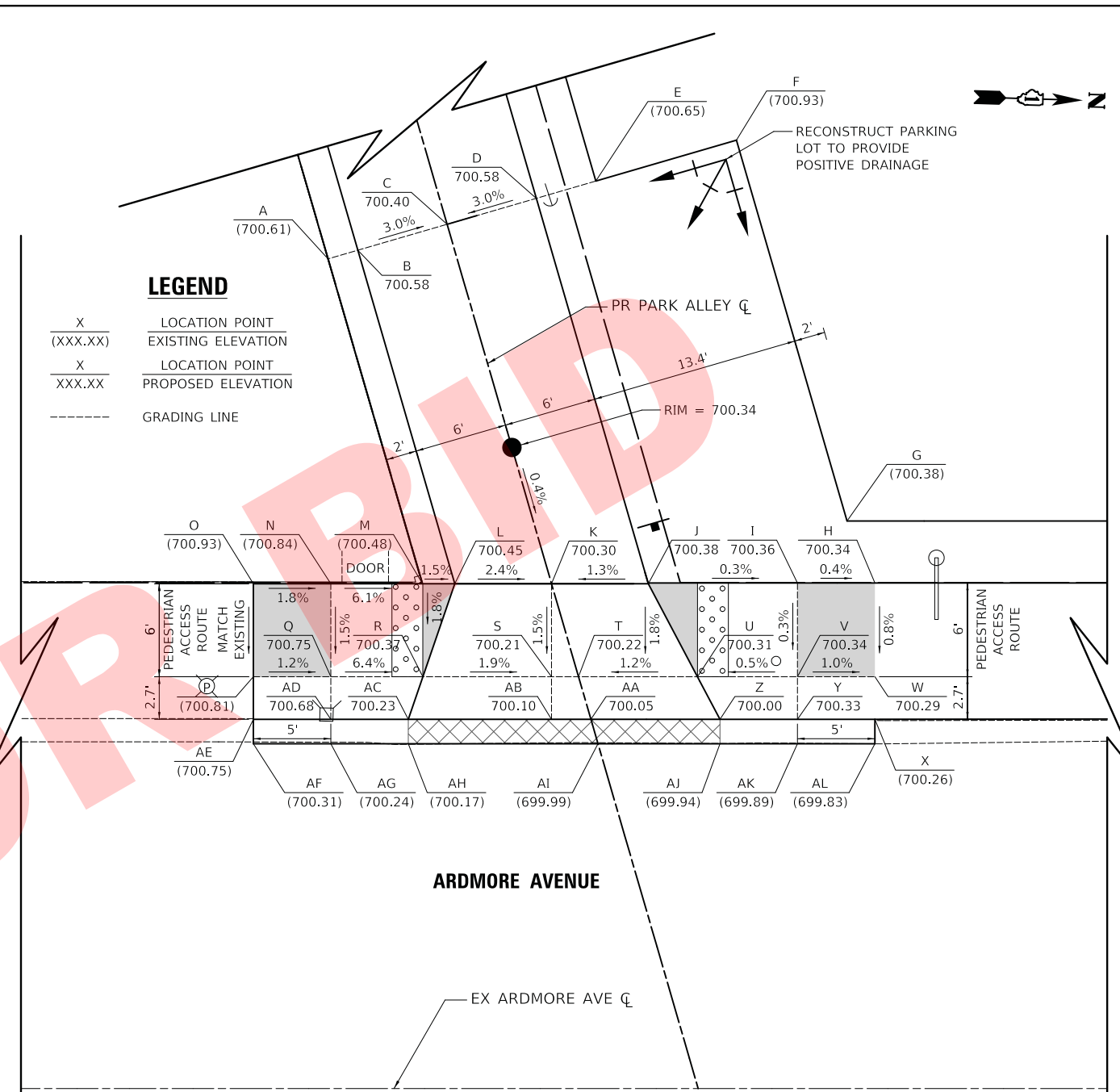


**EXAMPLE OF SIDEWALK GRADING DETAIL**

**NOTES**

ALL PROPOSED SIDEWALK GRADING AT INTERSECTIONS SHALL BE IN ACCORDANCE WITH THE PUBLIC RIGHT-OF-WAY ACCESSIBILITY GUIDELINES (PROWAG), IDOT HIGHWAY STANDARD 424001-11, "PERPENDICULAR CURB RAMP FOR SIDEWALKS" AND THE FOLLOWING NOTES:

1. ALL TRANSVERSE AND LONGITUDINAL SLOPES OF THE LANDING/TURNING SPACE AREA SHALL NOT EXCEED 1.5%. IN CERTAIN SITUATIONS, ONLY THE ENGINEER SHALL ALLOW SLOPES STEEPER THAN 1.5% BUT NOT TO EXCEED 2%.
2. IN BETWEEN LANDING/TURNING SPACE AREAS, IT IS RECOMMENDED TO MAXIMIZE THE LONGITUDINAL SLOPE IN ORDER TO GAIN HEIGHT WITH THE SIDEWALK. THIS IS PARTICULARLY BENEFICIAL WHEN A RUNNING SLOPE OF LESS THAN 5% IS NOT ABLE TO BE ACHIEVED BEYOND THE TURNING SPACE. WHEN THE SLOPE EXCEEDS 5%, THERE MUST BE AN UPPER AND LOWER LANDING WITHIN 15' OF EACH OTHER. THE SLOPE IN BETWEEN THE LANDINGS SHALL NOT EXCEED 8%.
3. THE LONGITUDINAL SLOPE BEYOND THE LANDING/TURNING SPACE AREA SHALL NOT EXCEED 4.9%.
4. THE TRANSITION SQUARE (LAST SQUARE SHOWN ON THE ROADWAY PLANS) SHALL MATCH INTO THE EXISTING SIDEWALK CONDITIONS REGARDLESS OF CONFORMANCE TO PROWAG GUIDELINES. THE NON-CONFORMING AREAS BEYOND THE INTERSECTION IMPROVEMENTS WILL BE ADDRESSED AS PART OF THE VILLAGE-WIDE SIDEWALK PROGRAM.
5. DEPENDING ON THE GRADING CONDITIONS OF THE ROADWAY AND OTHER CONSTRAINTS, THERE MAY BE SITUATIONS WHERE THE SIDEWALK RAMP WILL PITCH BACKWARDS TOWARDS THE PARKWAY. IN THESE SITUATIONS, THE RUNOFF MUST ALWAYS FLOW TOWARDS THE ROADWAY USING THE OTHER SIDEWALK RAMP.
6. THE SLOPE ALONG THE SIDEWALK CURB RAMP SHALL NOT EXCEED 1.5%. THIS MAY REQUIRE ADDITIONAL HMA SURFACE REMOVAL OR ADDITIONAL LEVELING BINDER TO HAVE THE PROPOSED HMA SURFACE COURSE MEET THE EDGE-OF-PAVEMENT.



**PARK ALLEY RETURN GRADING DETAILS**

PARK ALLEY (EAST END)		
POINT	STATION	OFFSET
A	14+00.00	8.00 RT
B	14+00.00	6.00 RT
C	14+00.00	0.00 RT
D	14+00.00	6.00 LT
E	14+00.00	10.00 LT
F	14+00.00	19.45 LT
G	14+25.59	19.45 LT
H	14+29.94	20.06 LT
I	14+28.54	15.26 LT
J	14+25.90	6.00 LT

PARK ALLEY (EAST END)		
POINT	STATION	OFFSET
K	14+24.16	0.00 RT
L	14+22.42	6.00 RT
M	14+21.83	8.00 RT
J	14+20.18	13.68 RT
O	14+18.79	18.48 RT
P	14+24.55	20.16 RT
Q	14+25.94	15.36 RT
R	14+27.60	9.65 RT
S	14+29.92	1.67 RT
T	14+30.41	0.00 RT

PARK ALLEY (EAST END)		
POINT	STATION	OFFSET
U	14+32.55	7.37 LT
V	14+34.35	13.57 LT
W	14+35.75	18.37 LT
X	14+38.39	17.60 LT
Y	14+36.99	12.80 LT
Z	14+35.60	8.00 LT
AA	14+33.27	0.00 RT
AB	14+32.56	2.44 RT
AC	14+29.98	11.32 RT
AD	14+28.59	16.12 RT

PARK ALLEY (EAST END)		
POINT	STATION	OFFSET
AE	14+27.19	20.93 RT
AF	14+28.71	21.37 RT
AG	14+30.10	16.56 RT
AH	14+31.50	11.76 RT
AI	14+34.92	0.00 RT
AJ	14+37.11	7.56 LT
AK	14+38.51	12.36 LT
AL	14+39.90	17.16 LT

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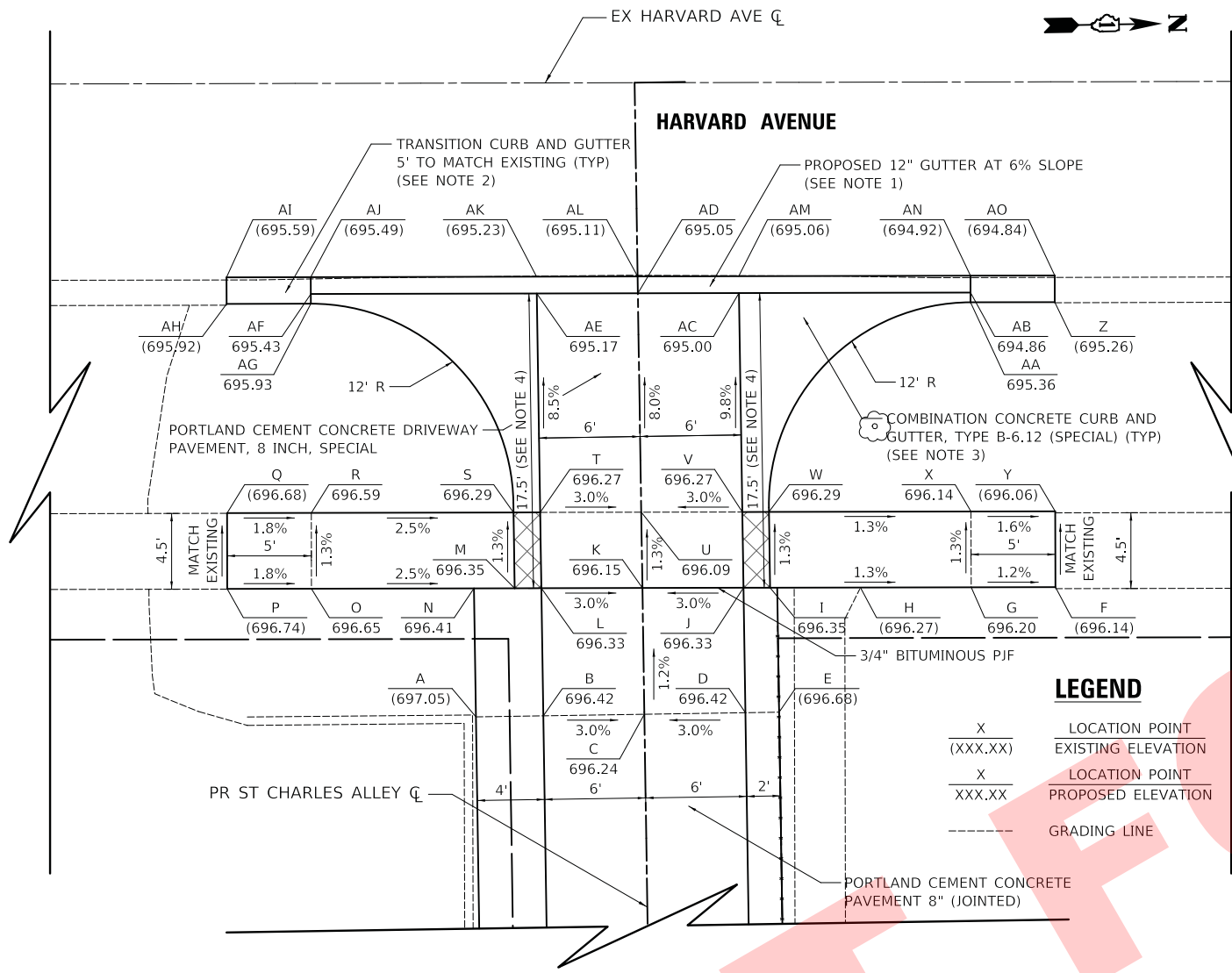
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**VILLAGE OF VILLA PARK**

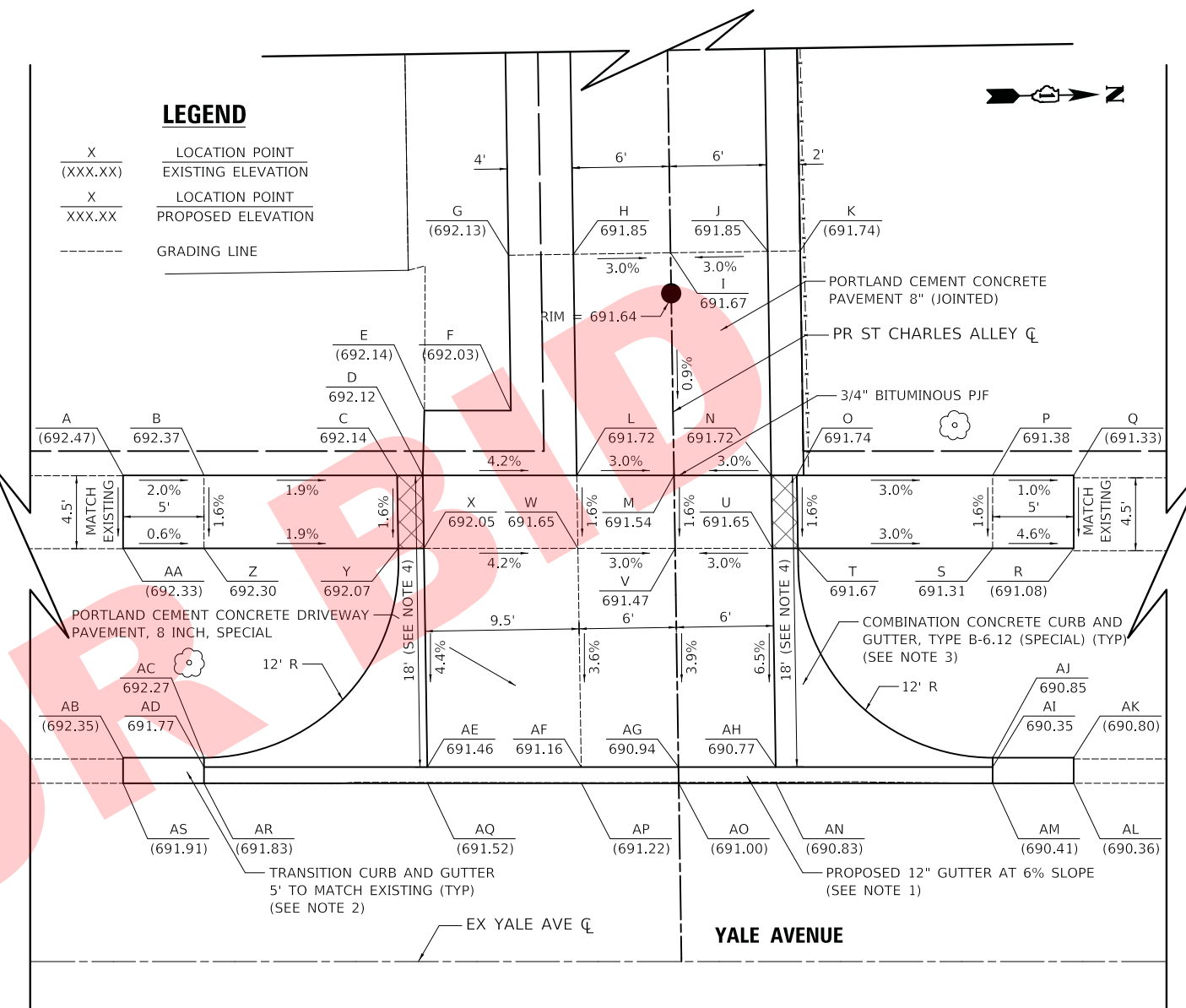
**2019 STREET IMPROVEMENT PROJECT  
GRADING DETAILS**

SCALE: 1" = 5' SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DUPAGE	37	27
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				



**ST. CHARLES ALLEY RETURN GRADING DETAILS (WEST END)**



**ST. CHARLES ALLEY RETURN GRADING DETAILS (EAST END)**

POINT	STATION	OFFSET
A	30+37.50	10.00 RT
B	30+37.50	6.00 RT
C	30+37.50	0.00 RT
D	30+37.50	6.00 LT
E	30+37.50	8.00 LT
F	30+30.33	24.51 LT
G	30+30.26	19.51 LT
H	30+30.17	12.94 LT
I	30+30.10	7.58 LT
J	30+30.08	6.00 LT
K	30+30.00	0.00 RT
L	30+29.92	6.00 RT
M	30+29.90	7.58 RT
N	30+29.87	10.00 RT
O	30+29.74	19.65 RT

POINT	STATION	OFFSET
P	30+29.68	24.64 RT
Q	30+25.18	24.59 RT
R	30+25.24	19.59 RT
S	30+25.40	7.58 RT
T	30+25.42	6.00 RT
U	30+25.50	0.00 RT
V	30+25.58	6.00 LT
W	30+25.60	7.58 LT
X	30+25.76	19.57 LT
Y	30+25.83	24.57 LT
Z	30+13.41	24.74 LT
AA	30+13.34	19.74 LT
AB	30+12.76	19.75 LT
AC	30+12.58	6.00 LT
AD	30+12.50	0.00 RT

POINT	STATION	OFFSET
AE	30+12.42	6.00 RT
AF	30+12.24	19.41 RT
AG	30+12.82	19.42 RT
AH	30+12.76	24.42 RT
AI	30+11.18	24.40 RT
AJ	30+11.24	19.40 RT
AK	30+11.42	6.00 RT
AL	30+11.50	0.00 RT
AM	30+11.58	6.00 LT
AN	30+11.76	19.76 LT
AO	30+11.83	24.76 LT

POINT	STATION	OFFSET
A	34+25.71	34.00 RT
B	34+25.79	29.00 RT
C	34+25.97	17.08 RT
D	34+26.00	15.50 RT
E	34+22.00	15.34 RT
F	34+22.08	10.00 RT
G	34+12.50	10.00 RT
H	34+12.50	6.00 RT
I	34+12.50	0.00 RT
J	34+12.50	6.00 LT
K	34+12.50	8.00 LT
L	34+26.14	6.00 RT
M	34+26.24	0.00 RT
N	34+26.33	6.00 LT
O	34+26.35	7.58 LT

POINT	STATION	OFFSET
P	34+26.54	19.66 LT
Q	34+26.62	24.60 LT
R	34+31.12	24.59 LT
S	34+31.04	19.59 LT
T	34+30.85	7.58 LT
U	34+30.83	6.00 LT
V	34+30.74	0.00 RT
W	34+30.64	6.00 RT
X	34+30.50	15.50 RT
Y	34+30.47	17.08 RT
Z	34+30.29	29.07 RT
AA	34+30.21	34.07 RT
AB	34+43.13	34.26 RT
AC	34+43.21	29.27 RT
AD	34+43.79	29.27 RT

POINT	STATION	OFFSET
AE	34+44.00	15.50 RT
AF	34+44.15	6.00 RT
AG	34+44.24	0.00 RT
AH	34+44.33	6.00 LT
AI	34+44.54	19.39 LT
AJ	34+43.96	19.40 LT
AK	34+44.03	24.39 LT
AL	34+45.61	24.37 LT
AM	34+45.54	19.37 LT
AN	34+45.33	6.00 LT
AO	34+45.24	0.00 RT
AP	34+45.15	6.00 RT
AQ	34+45.00	15.50 RT
AR	34+44.79	29.29 RT
AS	34+44.71	34.29 RT

**NOTES**

- THE 12" GUTTER WILL BE CONSTRUCTED PRIOR TO POURING ANY OF THE ALLEY RETURN PAVEMENT. THE GUTTER WILL BE PAID FOR AS: COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 (SPECIAL)
- TRANSITIONING THE CURB AND GUTTER TO MATCH THE EXISTING CURB AND GUTTER WILL BE PAID FOR AS: COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 (SPECIAL)
- ONCE THE RECTANGULAR DRIVEWAY ALLEY PAVEMENT HAS BEEN POURED, THE CURB AND GUTTER WILL BE POURED TO THE DIMENSIONS SHOWN IN THE DETAILS.
- THE CURB AND GUTTER WILL BE MEASURED FOR PAYMENT ALONG THE LONGITUDINAL EDGE OF THE DRIVEWAY ALLEY PAVEMENT REGARDLESS OF THE AMOUNT OF CONCRETE NEEDED TO COMPLETE THE RADII OF THE BACK OF CURBS.

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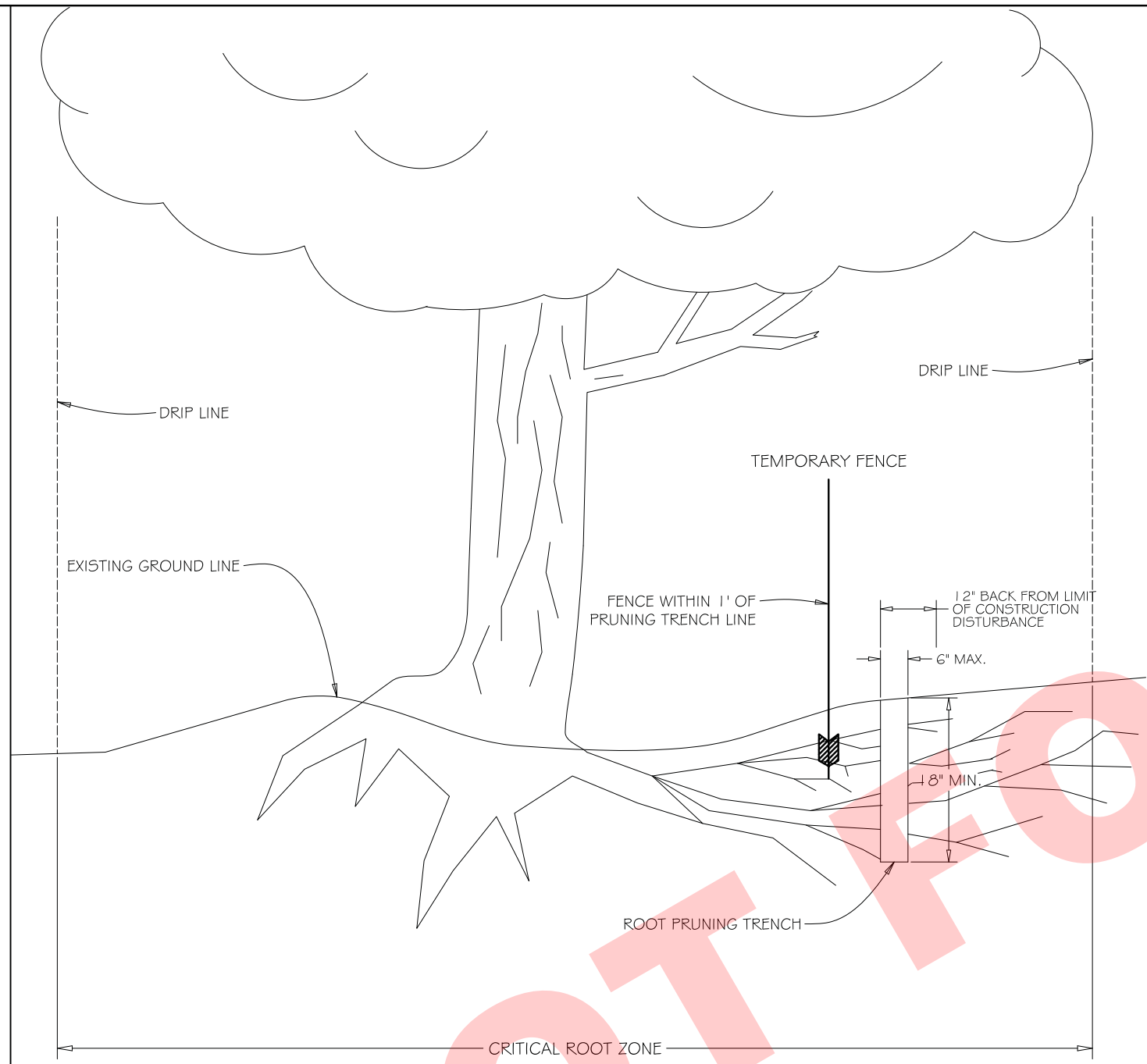
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**VILLAGE OF VILLA PARK**

**2019 STREET IMPROVEMENT PROJECT  
 GRADING DETAILS**

SCALE: 1" = 5' SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			37	28
CONTRACT NO.				
ILLINOIS		FED. AID PROJECT		

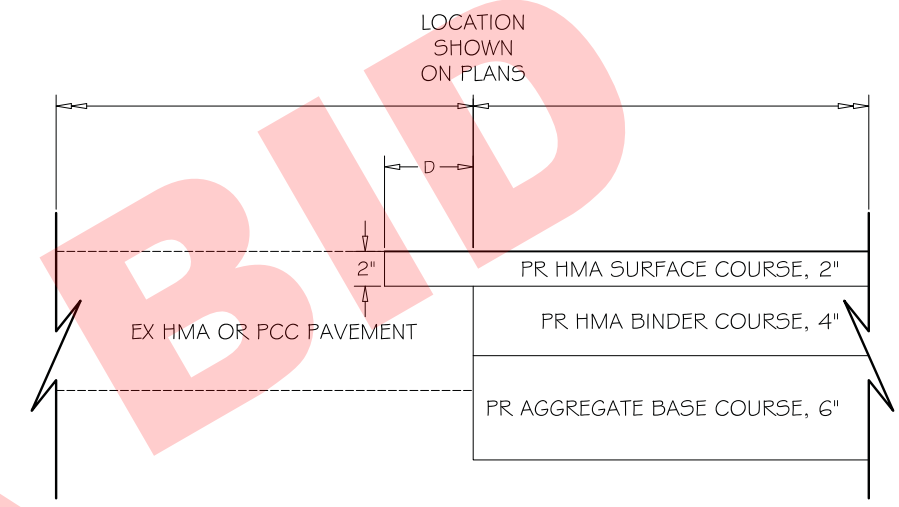


**GENERAL NOTES:**

1. EXACT LOCATION OF ROOT PRUNING TO BE AS SPECIFIED BY ENGINEER OR VILLAGE REPRESENTATIVE.
2. BACKFILL ROOT PRUNING TRENCH IMMEDIATELY FOLLOWING COMPLETION OF PRUNING ACTIVITIES.
3. ROOT PRUNING ACTIVITIES AND THE PLACEMENT OF PROTECTIVE FENCING SHALL BE COORDINATED SO THAT BOTH SHALL BE COMPLETED AT EACH LOCATION WITHIN A 48 HOUR PERIOD.
4. UNDER NO CIRCUMSTANCES SHALL CONSTRUCTION PROCEED PRIOR TO THE COMPLETION OF ROOT PRUNING AND THE PLACEMENT OF PROTECTIVE FENCING, NOR SHALL FENCING BE REMOVED WITHOUT PRIOR AUTHORIZATION FROM THE ENGINEER OR VILLAGE REPRESENTATIVE.

NOT TO SCALE

**TREE AND BUSH ROOT PRUNING**



**PROPOSED HMA BUTT JOINT**

**GENERAL NOTES:**

1. SEE ROADWAY PLANS FOR LOCATION OF BUTT JOINT AND DIMENSION "D".
2. THE 2" REMOVAL OF EXISTING HMA SURFACE COURSE SHALL BE PAID FOR AS "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT".
3. THE 2" REMOVAL OF EXISTING PCC SURFACE PAVEMENT SHALL BE PAID FOR AS "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".
4. FOR RESURFACING AREAS AND FOR TEMPORARY RAMP, SEE IDOT DISTRICT ONE DETAIL BD-32 "BUTT JOINT AND HMA TAPER DETAILS".

NOT TO SCALE

**HOT-MIX ASPHALT BUTT JOINT (RECONSTRUCTION AREAS)**

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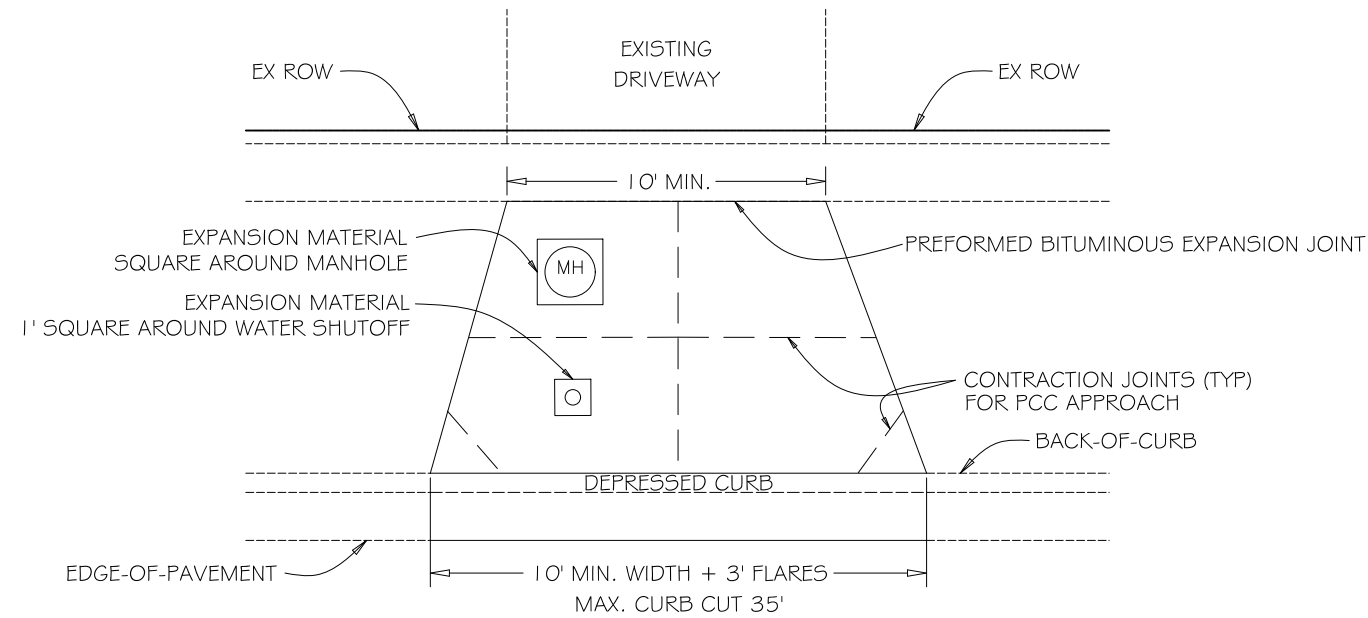
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**VILLAGE OF VILLA PARK**

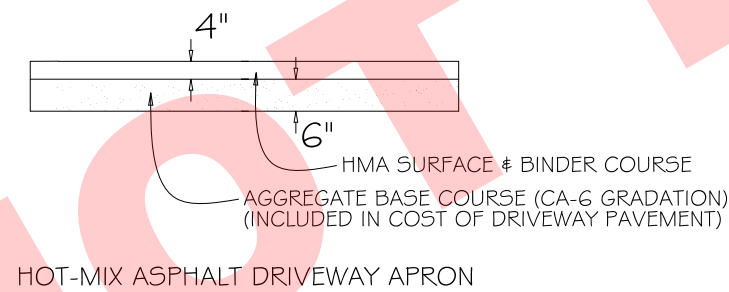
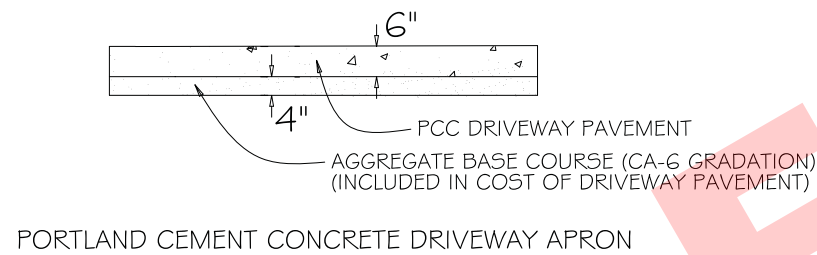
**2019 STREET IMPROVEMENT PROJECT  
CONSTRUCTION DETAILS**

SCALE: N.T.S. SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DUPAGE	37	29
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				



PLAN



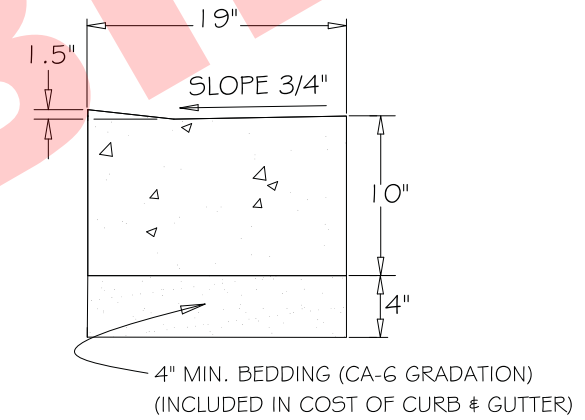
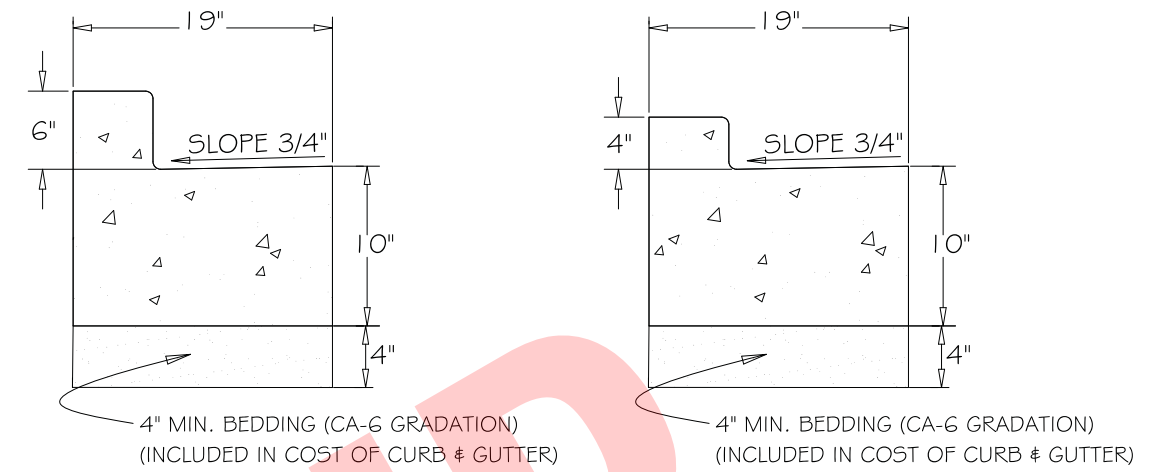
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GENERAL NOTES:

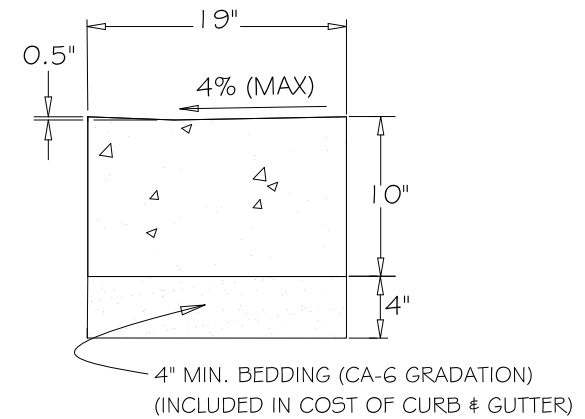
1. DEPRESSING THE CURB THROUGH DRIVEWAY APRON SHALL BE INCLUDED IN THE COST OF THE COMBINATION CONCRETE CURB AND GUTTER BEING CONSTRUCTED.
2. ALL STONE BEDDING SHALL BE MECHANICALLY COMPACTED.
3. SEE "COMBINATION CONCRETE CURB AND GUTTER" DETAIL FOR CURB AND GUTTER CONSTRUCTION.

NOT TO SCALE

TYPICAL DRIVEWAY APPROACH



B-4.12 & B-6.12 (DEPRESSED)



CURB RAMPS (DEPRESSED)

GENERAL NOTES:

1. CONTRACTION JOINTS SHALL BE PLACED BETWEEN EXPANSION JOINTS AT 15' INTERVALS. CONTRACTION JOINTS SHALL INITIALLY BE TOOLED AND THEN SAW CUT WITHIN 24 HOURS OF POURING THE CURB AND GUTTER.
2. EXPANSION JOINTS ARE TO BE PLACED AT 60' INTERVALS. 2 - #6 X 18" REBARS TO BE PLACED AT ALL EXPANSION JOINTS.
3. SEE IDOT HIGHWAY STANDARD G6001 FOR ADDITIONAL CURB AND GUTTER DETAILS.
4. ALL CURB AND GUTTER TYPES SHALL BE PAID FOR AS "COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 (SPECIAL)".

NOT TO SCALE

COMBINATION CONCRETE CURB AND GUTTER

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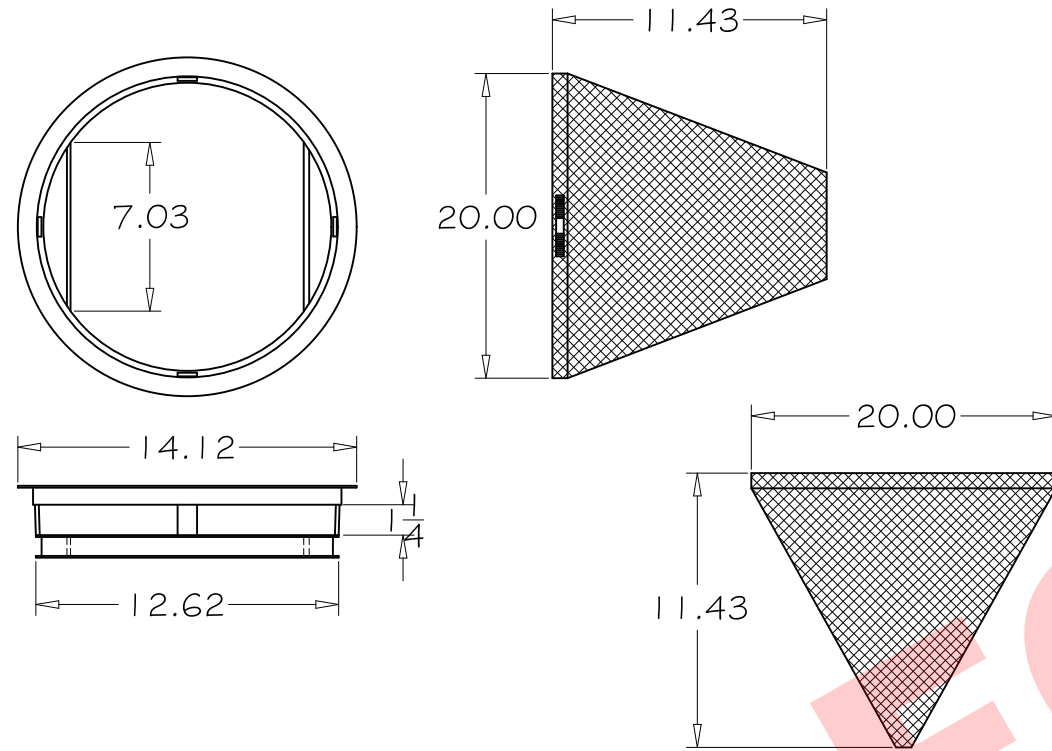
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VILLAGE OF VILLA PARK

2019 STREET IMPROVEMENT PROJECT  
CONSTRUCTION DETAILS

SCALE: N.T.S. SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			37	30
			CONTRACT NO.	
ILLINOIS FED. AID PROJECT				



DRAWING BY MARATHON MATERIALS, INC.

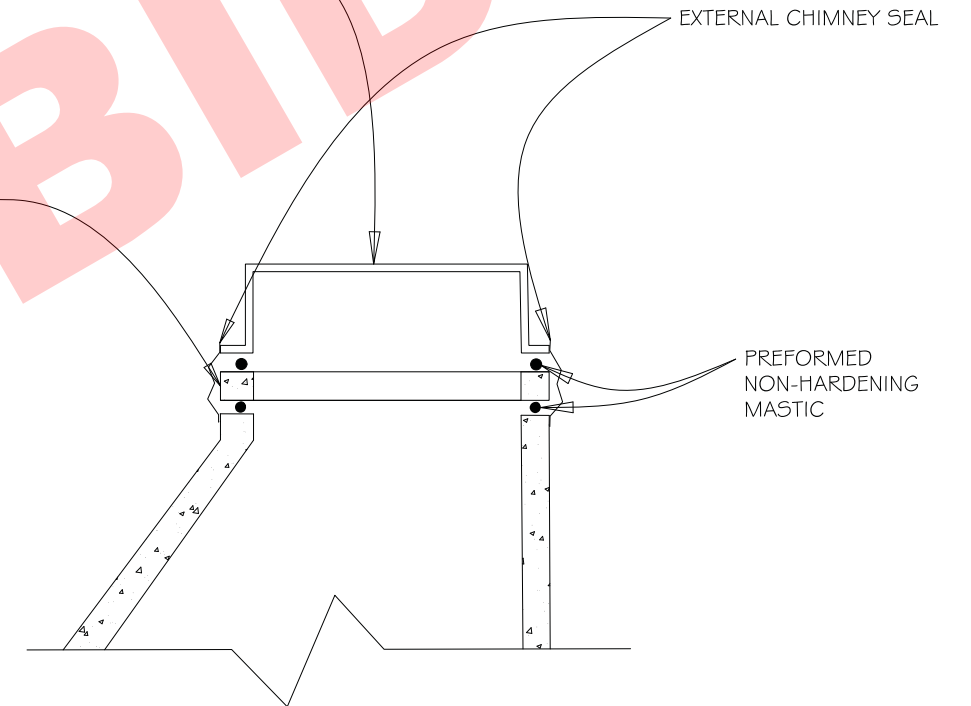
**GENERAL NOTES:**

1. USE DRAINAGE STRUCTURE INLET FILTER MANUFACTURED BY MARATHON MATERIALS, INC. OR APPROVED EQUAL.
2. FRAME: TOP FLANGE FABRICATED FROM 1.25 X 1.25 X 0.125 ANGLE. BASE RIM FABRICATED FROM 1.5 X 0.5 X 0.125 CHANNEL. HANDLES AND SUSPENSION BRACKETS FABRICATED FROM 1.25 X 0.25 FLAT STOCK. ALL DOMESTIC STEEL CONFORMING TO ASTM - A36.
3. SEDIMENT BAG: BAG FABRICATED FROM 4 OZ./SQ. YD. NON-WOVEN POLYPROPYLENE GEOTEXTILE REINFORCED WITH POLYESTER MESH. BAG SECURED TO BASE RIM WITH A STAINLESS STEEL STRAP AND LOCK.

NOT TO SCALE

EXISTING FRAME AND LID TO BE ADJUSTED OR NEW FRAME AND LID (WHEN SPECIFIED)

2" MIN. TO 12" MAX. ADJUSTMENT  
2 PRECAST ADJUSTING RINGS MAX.



**GENERAL NOTES:**

1. EXTERNAL MANHOLE CHIMNEY SEAL CONFORMING TO ASTM C-923 CRETEX OR APPROVED EQUAL (SANITARY AND COMBINED MANHOLES ONLY)
2. FRAME AND LID (SANITARY):  
NEENAH R-1713 (SELF SEALING LID)
3. FRAME AND LID (STORM):  
NEENAH R-1713

NOT TO SCALE

**TYPICAL ADJUSTMENT**

REV:	DATE:
REV: RMS	DATE: JUL 2004
DRAWN BY: VV	DATE: JAN 2003

**DRAINAGE STRUCTURE PROTECTION**

**VILLAGE OF VILLA PARK  
STORM - 17**

**VILLAGE OF VILLA PARK**

**2019 STREET IMPROVEMENT PROJECT  
CONSTRUCTION DETAILS**

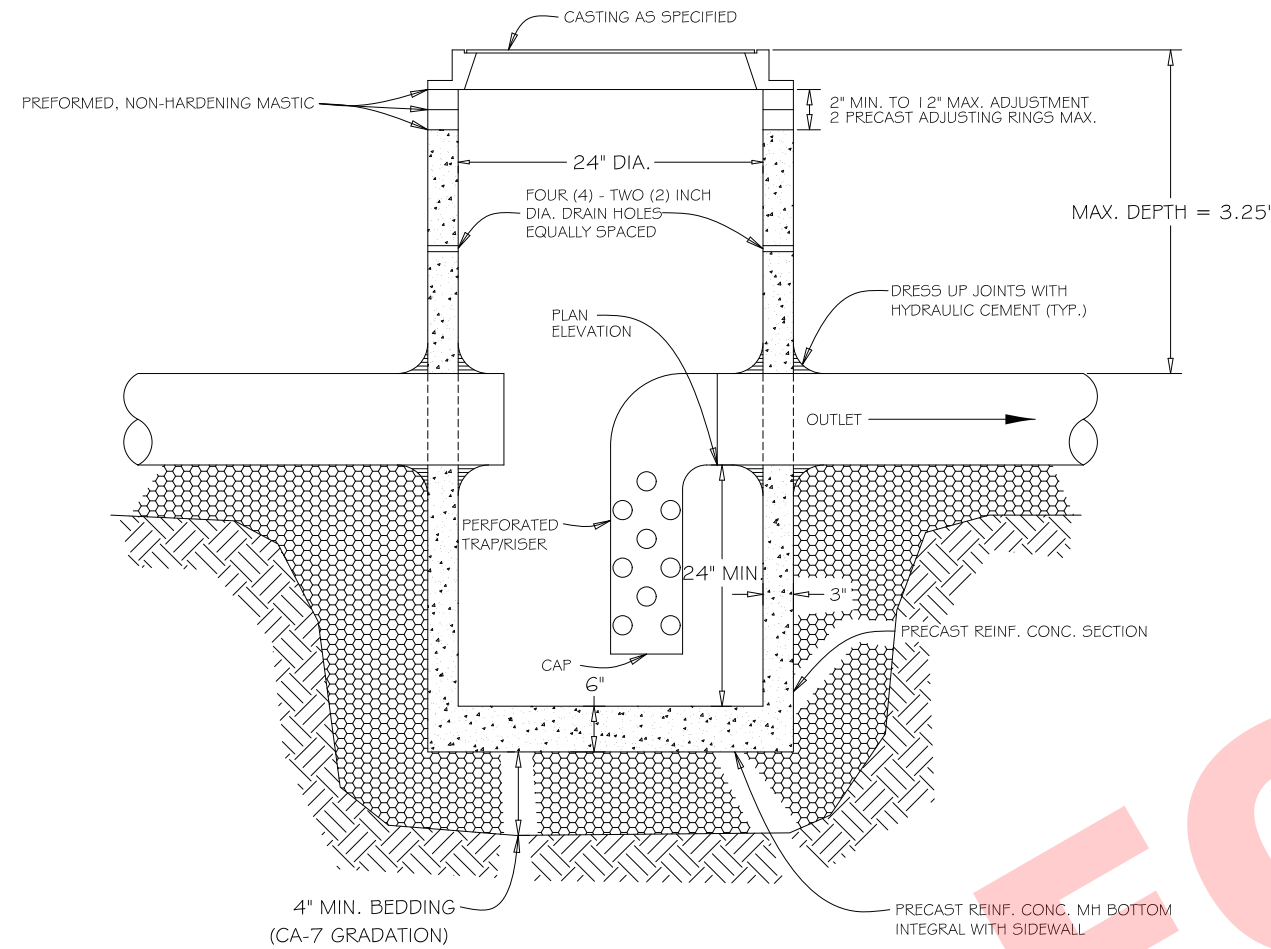
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			37	31
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

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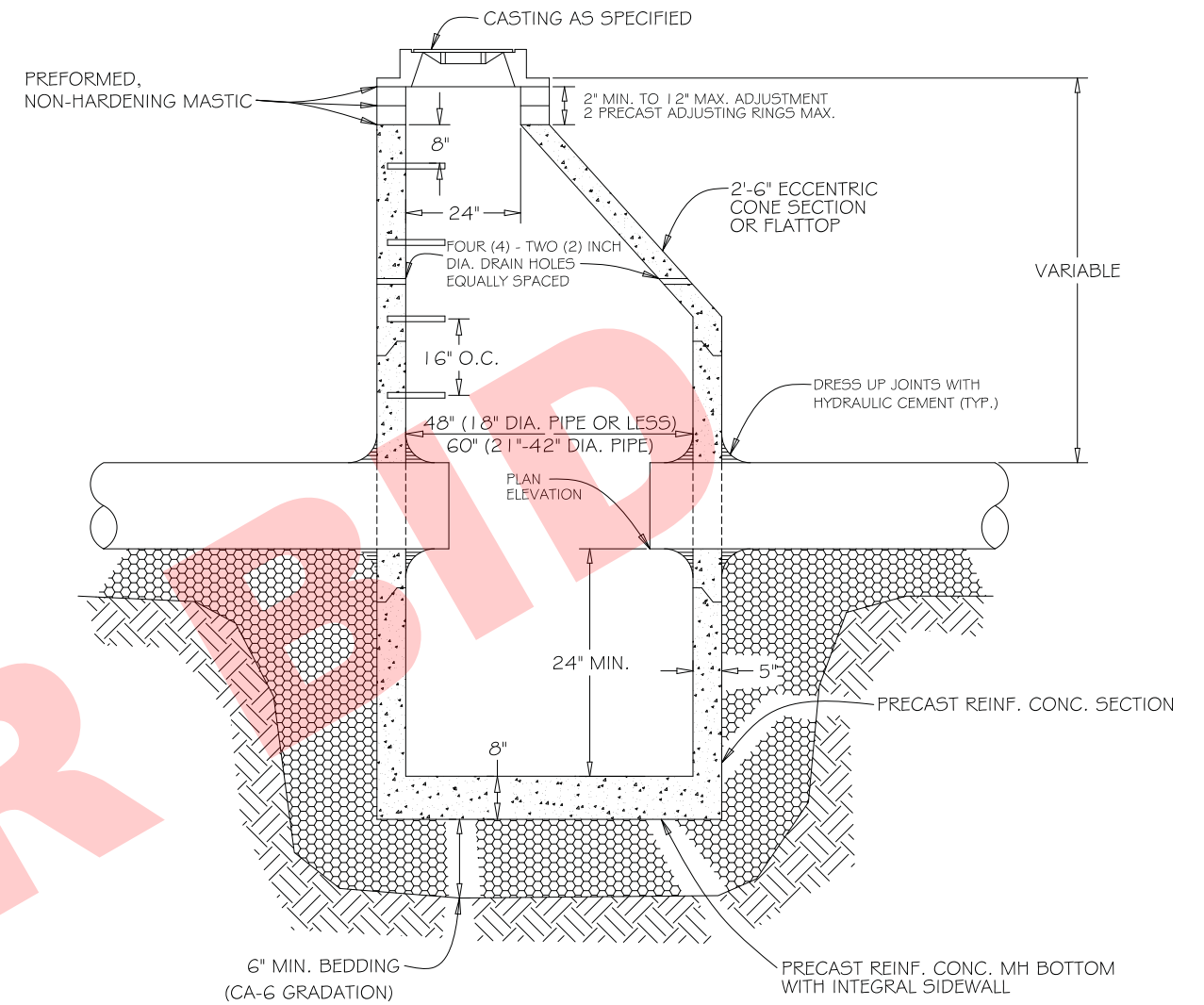


**GENERAL NOTES:**

1. PROVIDE PRECAST REINFORCED CONCRETE BARREL AND RISER SECTION. CONCRETE BLOCK CONSTRUCTION IS NOT PERMITTED.
2. PROVIDE GRANULAR BACKFILL AROUND CATCH BASIN TO SUBGRADE ELEVATION IN PAVED AREAS. MATERIAL SHALL MEET THE REQUIREMENTS OF IDOT "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" FOR COARSE AGGREGATE (CA-6 GRADATION.)
3. WHEN THE FRAME DOES NOT MEET PROPOSED ELEVATION, A MAXIMUM OF TWO PRECAST CONCRETE RINGS MAY BE USED TO A MAXIMUM HEIGHT OF 12 INCHES. THE RING(S) AND FRAME SHALL BE SET IN A BED OF PREFORMED NON-HARDENING MASTIC (RUB-R-NEK, EZ STICK OR APPROVED EQUAL).
4. PRECAST ADJUSTING RINGS SHALL BE REINFORCED WITH NO. 3 GAUGE WIRE OR EQUIVALENT AND SHALL HAVE A MINIMUM THICKNESS OF TWO INCHES.
5. MORTAR SHALL NOT BE USED TO DRESS UP ADJUSTING RINGS AND/OR FRAME.
6. DRESS UP INTERIOR JOINTS WITH HYDRAULIC CEMENT.
7. IN PAVED AREAS, DRAIN HOLES/WEEP HOLES SHALL BE COVERED WITH FILTER FABRIC. FILTER FABRIC SHALL BE SECURED TO THE OUTSIDE OF STRUCTURE PRIOR TO BACKFILL.
8. IN GRASSED AREAS, DRAIN HOLES/WEEP HOLES SHALL BE PLUGGED WITH HYDRAULIC CEMENT.

NOT TO SCALE

**CATCH BASIN TYPE "C"**



**GENERAL NOTES:**

1. PROVIDE PRECAST REINFORCED CONCRETE BARREL AND RISER SECTION. CONCRETE BLOCK CONSTRUCTION IS NOT PERMITTED.
2. PROVIDE GRANULAR BACKFILL AROUND CATCH BASIN TO SUBGRADE ELEVATION IN PAVED AREAS. MATERIAL SHALL MEET THE REQUIREMENTS OF IDOT "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" FOR COARSE AGGREGATE (CA-6 GRADATION.)
3. APPLY A CONTINUOUS LAYER OF NON-HARDENING PREFORMED BITUMINOUS MASTIC MATERIAL (RUB-R-NEK OR EZ STICK) TO EACH JOINT TO PREVENT INFLOW.
4. WHEN THE FRAME DOES NOT MEET PROPOSED ELEVATION, A MAXIMUM OF TWO PRECAST CONCRETE RINGS MAY BE USED TO A MAXIMUM HEIGHT OF 12 INCHES. THE RING(S) AND FRAME SHALL BE SET IN A BED OF PREFORMED NON-HARDENING MASTIC (RUB-R-NEK, EZ STICK OR APPROVED EQUAL).
5. PRECAST ADJUSTING RINGS SHALL BE REINFORCED WITH NO. 3 GAUGE WIRE OR EQUIVALENT AND SHALL HAVE A MINIMUM THICKNESS OF TWO INCHES.
6. MORTAR SHALL NOT BE USED TO DRESS UP ADJUSTING RINGS AND/OR FRAME. DRESS UP INTERIOR JOINTS WITH HYDRAULIC CEMENT.
7. ONLY PLASTIC POLYMER STEPS SHALL BE USED.
8. WHEN CATCH BASIN DEPTH IS OVER 12 FEET, THE THICKNESS OF THE PRECAST REINFORCED CONCRETE BASE SHALL BE A MINIMUM OF 10 INCHES. WHEN CATCH BASIN DEPTH IS LESS THAN 12 FEET, THE THICKNESS SHALL BE A MINIMUM OF 8 INCHES.
9. DRESS UP INTERIOR JOINTS OF PRECAST CATCHBASIN AND OPENINGS AROUND THE PIPES WITH HYDRAULIC CEMENT.
10. IN PAVED AREAS, DRAIN HOLES/WEEP HOLES SHALL BE COVERED WITH FILTER FABRIC. FILTER FABRIC SHALL BE SECURED TO THE OUTSIDE OF STRUCTURE PRIOR TO BACKFILL.
11. IN GRASSED AREAS, DRAIN HOLES/WEEP HOLES SHALL BE PLUGGED WITH HYDRAULIC CEMENT.

NOT TO SCALE

**CATCH BASIN TYPE "A"**

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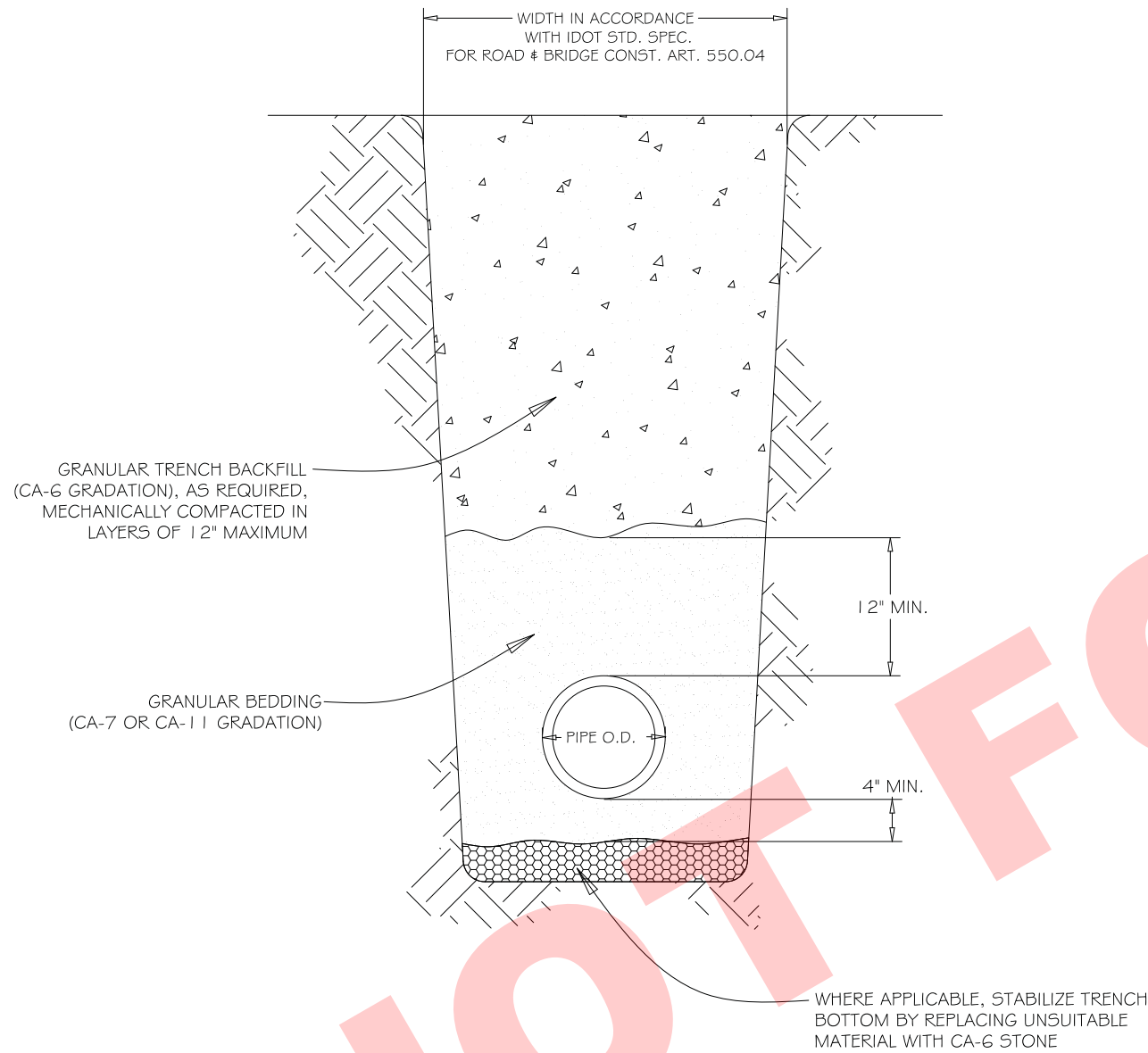
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**VILLAGE OF VILLA PARK**

**2019 STREET IMPROVEMENT PROJECT  
CONSTRUCTION DETAILS**

SCALE: N.T.S. SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			37	32
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

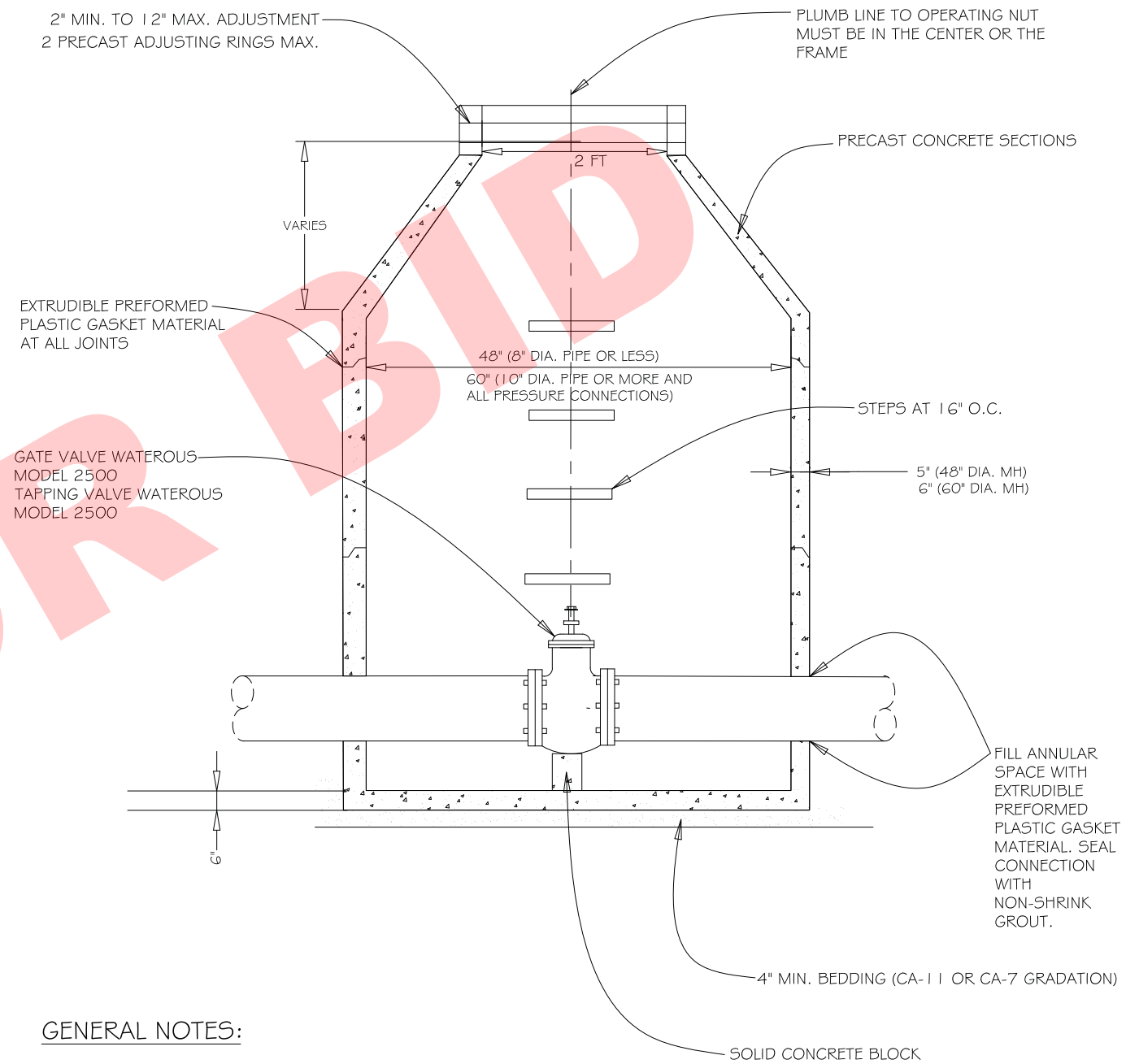


**GENERAL NOTES:**

1. GRANULAR TRENCH BACKFILL SHALL BE MECHANICALLY COMPACTED IN LAYERS OF 12" MAXIMUM, LOOSE MEASURE, TO 95% OF STANDARD MAXIMUM DENSITY (ASTM D 698)
2. TRENCH BACKFILL SHALL BE USED IN LOCATIONS SPECIFIED BY SECTION 208 OF THE STANDARD SPECIFICATIONS.

NOT TO SCALE

**PIPE INSTALLATION**



**GENERAL NOTES:**

1. ALL BELOW GRADE FASTENERS TO BE STAINLESS STEEL:
  - A. BOLTS AND THREADED RODS - GRADE #304
  - B. NUTS AND WASHERS - GRADE #300
2. FRAME AND LID:
  - A. NEENAH R-1713 (SELF SEALING)
  - B. "WATER" TO BE CAST INTO COVER

NOT TO SCALE

**WATER VALVE IN VAULT**

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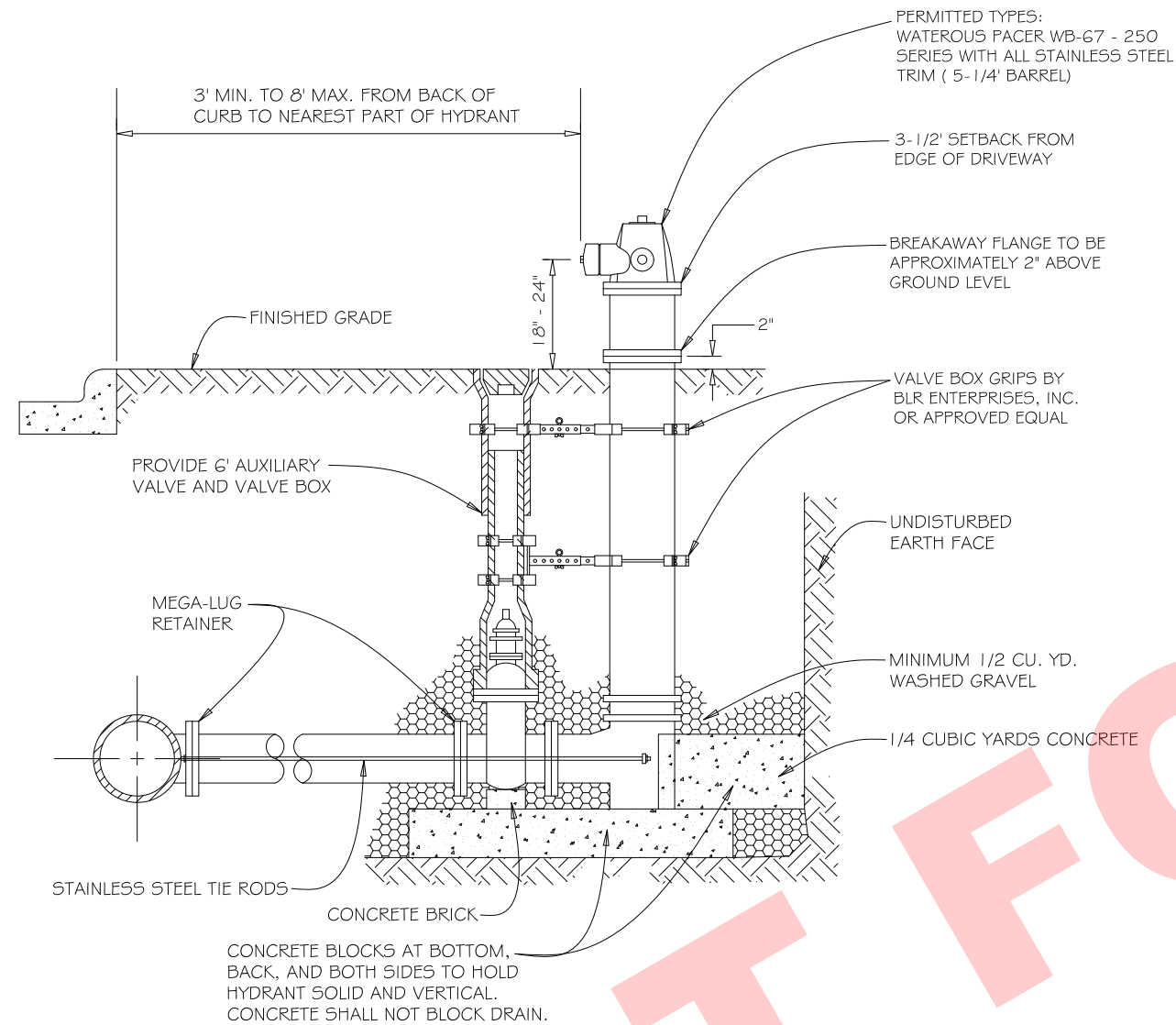
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**VILLAGE OF VILLA PARK**

**2019 STREET IMPROVEMENT PROJECT  
 CONSTRUCTION DETAILS**

SCALE: N.T.S. SHEET OF SHEETS STA. TO STA.

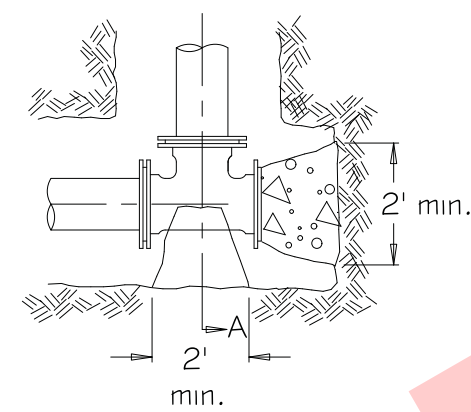
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			37	33
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				



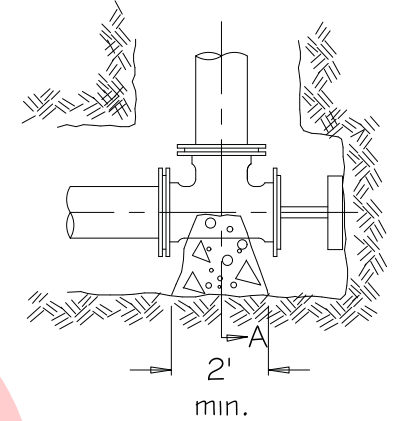
**GENERAL NOTES:**

1. MAXIMUM BARREL EXTENSIONS ARE 18 INCHES AND SHALL BE WATEROUS EXTENSION FOR WATEROUS HYDRANTS.
2. ALL HYDRANTS ARE TO BE SUPPLIED WITH A 6" FLANGED AND MECHANICAL JOINT AUXILIARY VALVE THAT CONFORMS TO AWWA 500-80. ALL TRIM BOLTS ARE TO BE STAINLESS STEEL.
3. ALL BELOW GRADE FASTENERS TO BE STAINLESS STEEL:
  - A. BOLTS AND THREADED RODS - GRADE #304
  - B. NUTS AND WASHERS - GRADE #300
4. MEGA-LUG RETAINERS MUST BE INSTALLED ON ALL MECHANICAL FITTINGS.

NOT TO SCALE



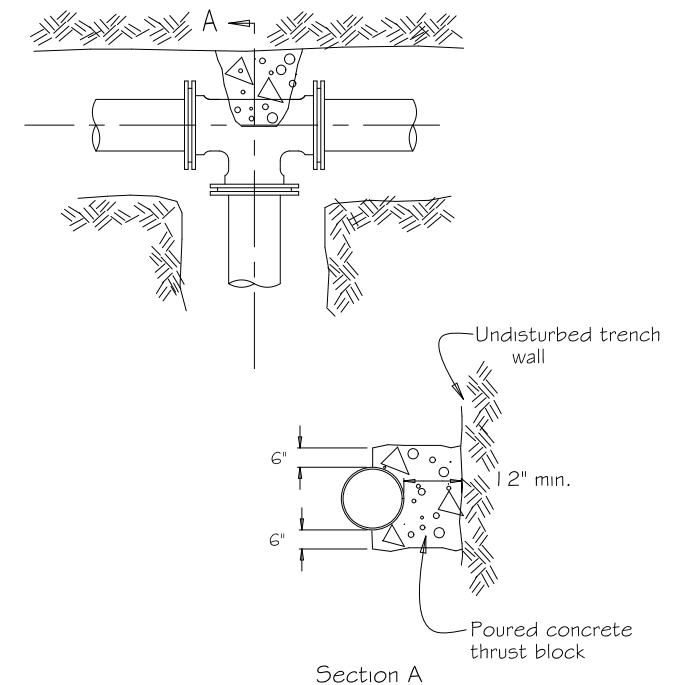
Alt. 1 Precast Block



Alt. 2 Brace Plug Against undisturbed earth

**GENERAL NOTES:**

1. Thrust blocks shall be made of 12" thick precast concrete blocks or poured in place concrete.
2. Thrust blocks shall be installed at all tees and bends of 11.25° and greater.
3. Thrust blocks shall be installed against undisturbed soil.
4. Concrete shall be 3000 PSI (min.).
5. Poured concrete shall be placed in such a manner that pipe and fittings will be accessible for repairs.
6. All joints requiring thrust blocking shall also use Meg-a-Lug retainer glands.
7. Use of wood materials for thrust blocking is strongly prohibited.
8. All below grade fasteners to be stainless steel:
  - bolts & threaded rods - grade 304
  - nuts & washers - grade 300



NOT TO SCALE

REV:	DATE:
REV: RMS	DATE: JUL 2004
DRAWN BY: VV	DATE: JAN 2003

**FIRE HYDRANT SETTING**

VILLAGE OF VILLA PARK  
WATER - 04

REV:	DATE:
REV: RMS	DATE: FEB 2005
DRAWN BY: VV	DATE: FEB 2005

**THRUST BLOCK INSTALLATION**

VILLAGE OF VILLA PARK  
WATER-22

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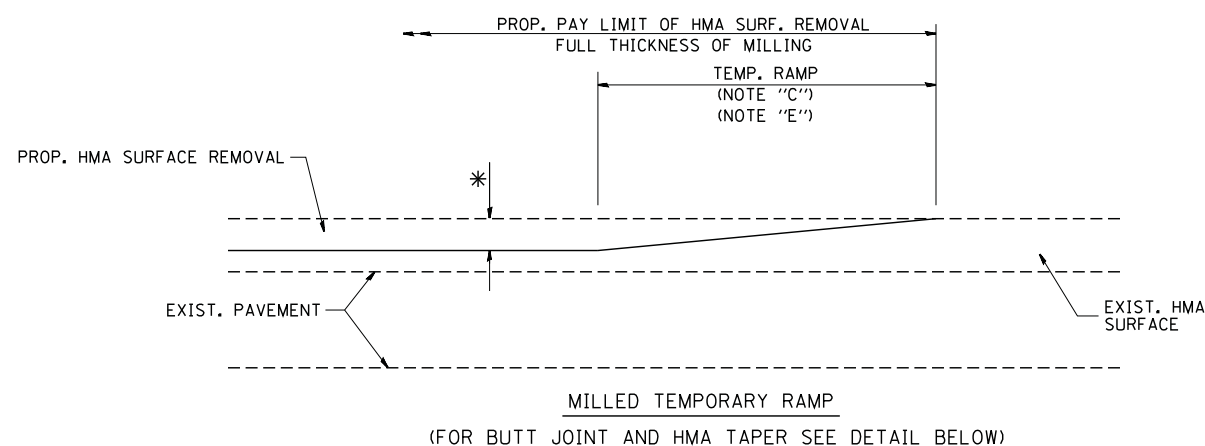
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	DATE - 06/21/19	REVISED -

VILLAGE OF VILLA PARK

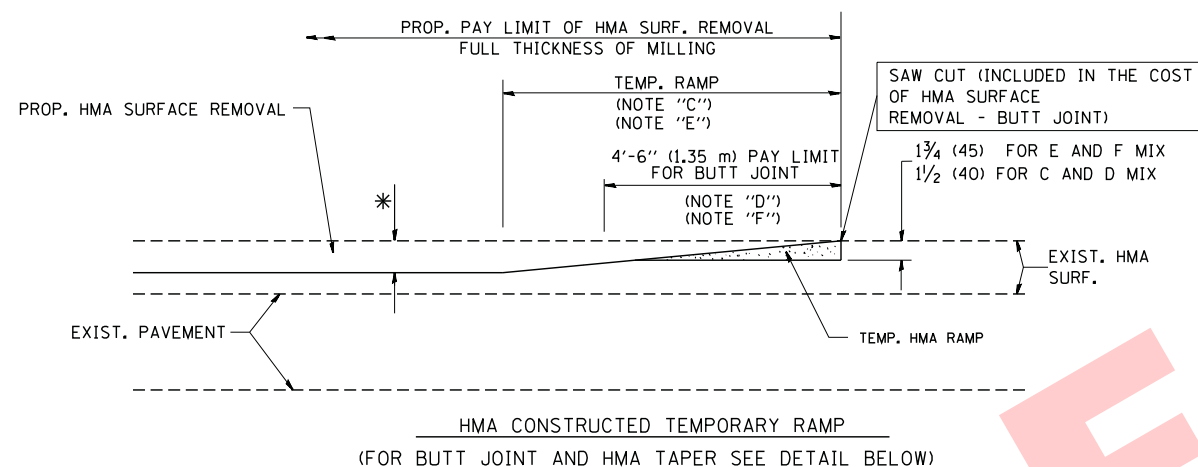
**2019 STREET IMPROVEMENT PROJECT  
CONSTRUCTION DETAILS**

SCALE: N.T.S. SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			37	34
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

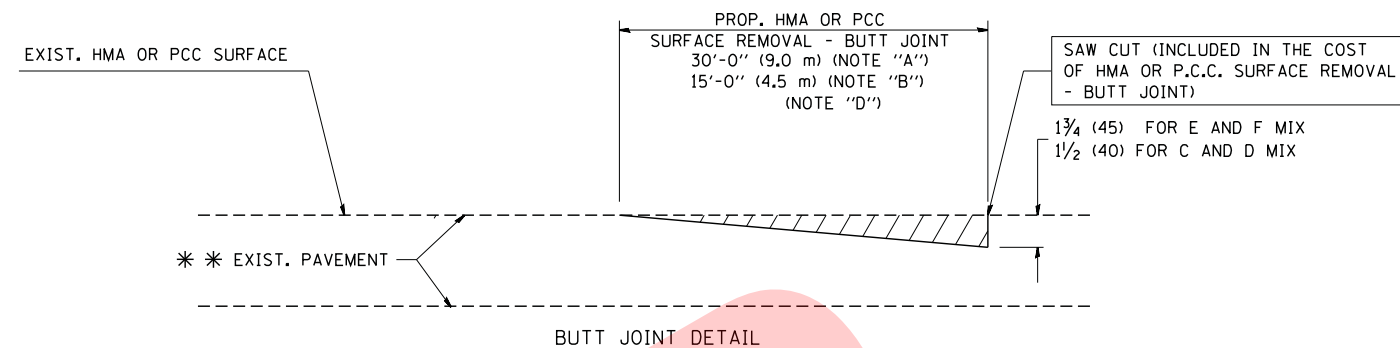


**OPTION 1**

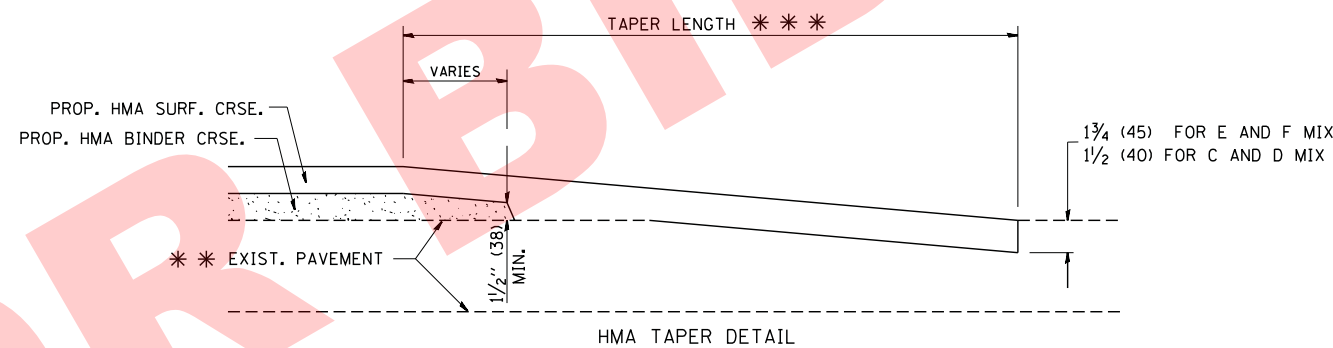


**OPTION 2**

**TYPICAL TEMPORARY RAMP**



**BUTT JOINT DETAIL**



**HMA TAPER DETAIL**

**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

**NOTES**

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

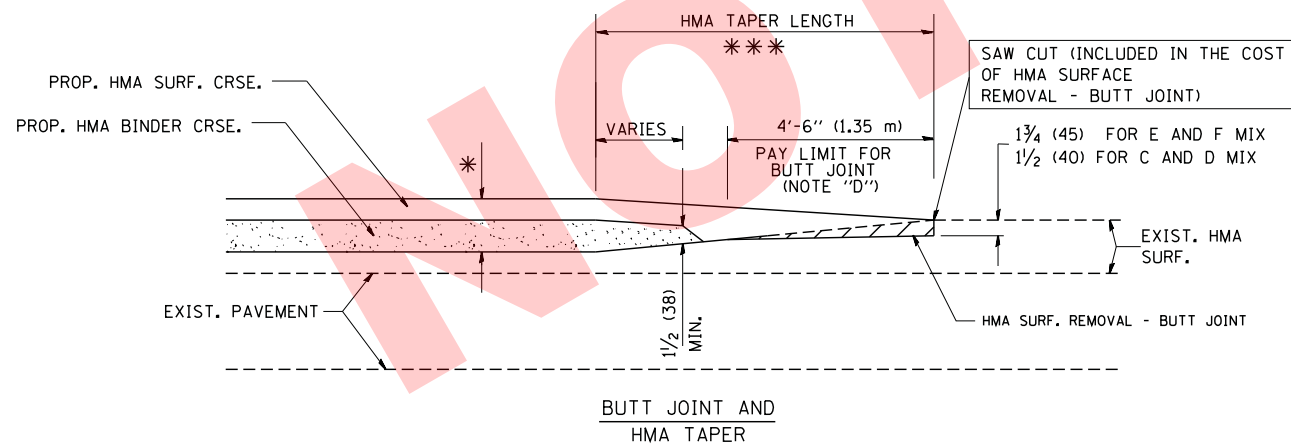
\* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

\*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT:**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**

FILE NAME = W:\diststd\22x34\bd32.dgn	USER NAME = gaglionobt	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

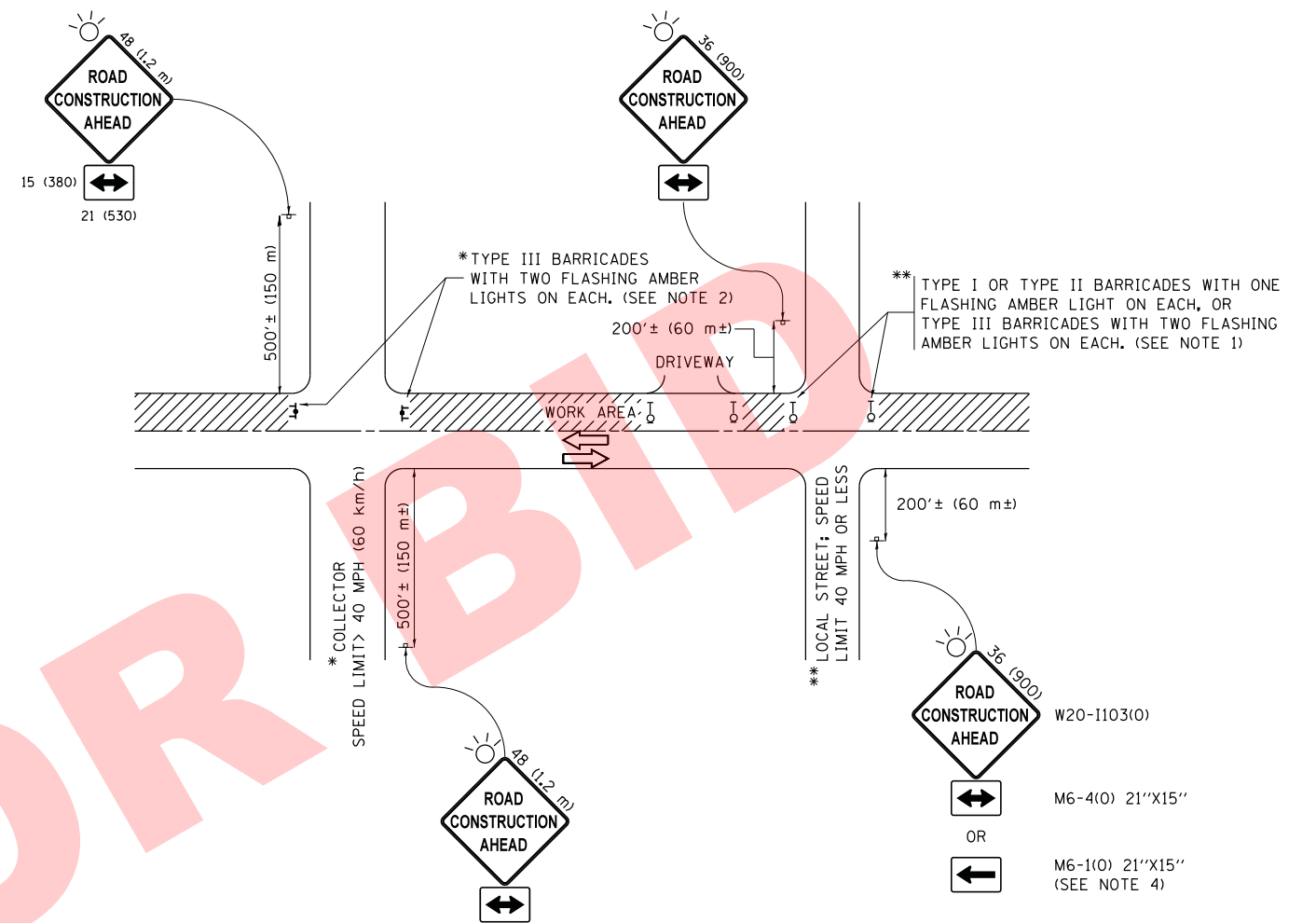
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND  
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DUPAGE	37	35
<b>BD400-05 BD32</b>		CONTRACT NO.		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

NOT FOR BIDDING



**NOTES:**

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

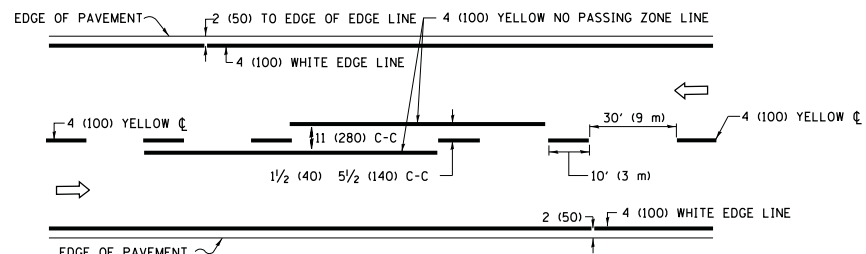
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = footemj	DESIGNED - L.H.A.	REVISED - A. HOUSEH 10-15-96
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Default	PLOT SCALE = 50.000' / in.	DATE - 06-89	REVISED - A. SCHUETZE 07-01-13
	PLOT DATE = 9/15/2016		REVISED - A. SCHUETZE 09-15-16

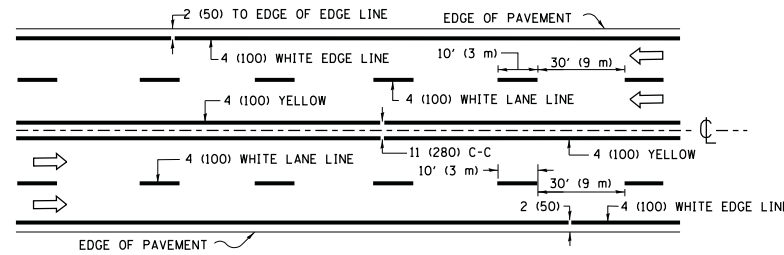
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS			
SCALE: NONE	SHEET 1	OF 1 SHEETS	STA. TO STA.

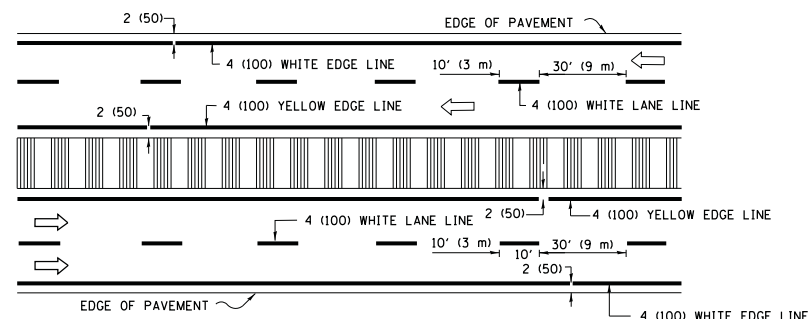
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			37	36
<b>TC-10</b>			<b>CONTRACT NO.</b>	
ILLINOIS FED. AID PROJECT				



**2-LANE ROADWAY**

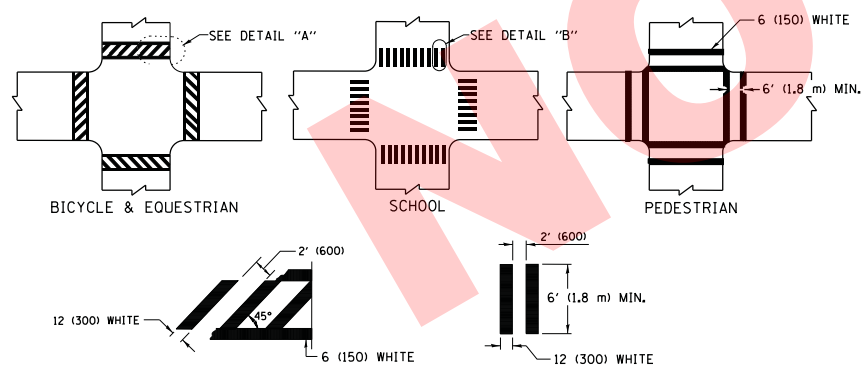


**MULTI-LANE UNDIVIDED**



**MULTI-LANE DIVIDED WITH MEDIAN**

**TYPICAL LANE AND EDGE LINE MARKING**

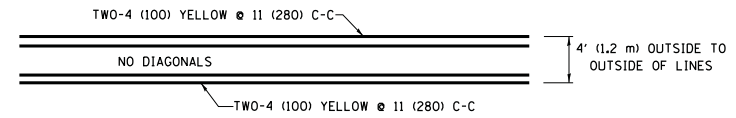


**DETAIL "A"**

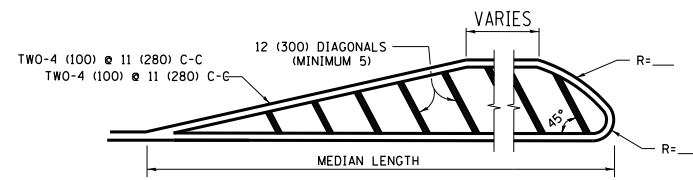
**DETAIL "B"**

**TYPICAL CROSSWALK MARKING**

\* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

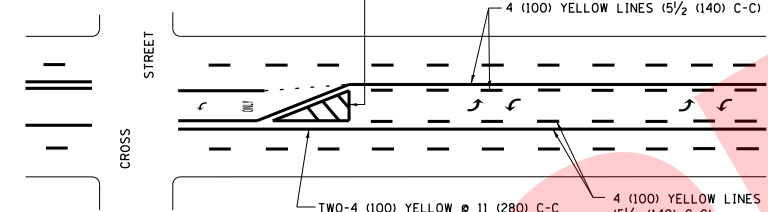


**4' (1.2 m) WIDE MEDIANS ONLY**



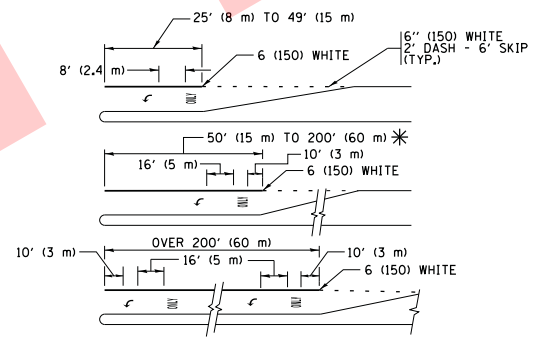
**MEDIANS OVER 4' (1.2 m) WIDE**

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))  
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)  
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))



**MEDIAN WITH TWO-WAY LEFT TURN LANE**

**TYPICAL PAINTED MEDIAN MARKING**

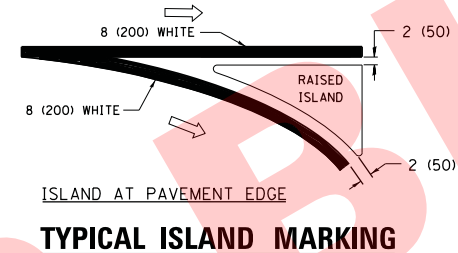
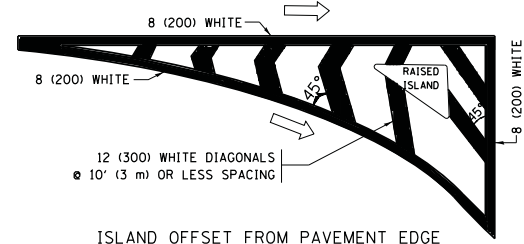


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)

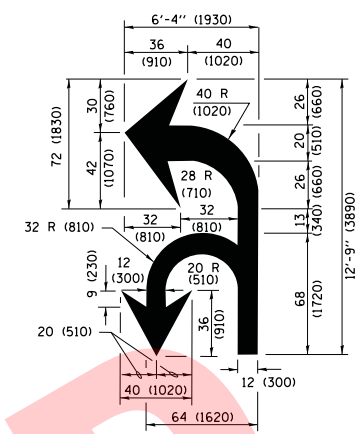
\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

**TYPICAL LEFT (OR RIGHT) TURN LANE**

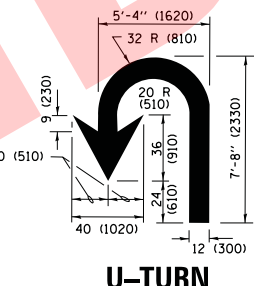
**TYPICAL TURN LANE MARKING**



**TYPICAL ISLAND MARKING**



**COMBINATION LEFT AND U-TURN**



**LANE REDUCTION TRANSITION**

\* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

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	PLOT DATE = 6/23/2017	CHECKED -	REVISED - C. JUCIUS 12-21-15
		DATE - 03-19-90	REVISED - C. JUCIUS 04-12-16

**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE TYPICAL PAVEMENT MARKINGS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	TC-13	DUPAGE	37	37
		CONTRACT NO.		
ILLINOIS FED. AID PROJECT				